

**CITY OF CHICAGO - DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY REPORT**

**Structure No. 016-6038**

**95<sup>th</sup> Street  
over  
the Calumet River**

**Inspection Date: August 11, 2021**



A Joint Venture Teaming of Alfred Benesch &  
Company and Collins Engineers, Inc.

## EXECUTIVE SUMMARY

On August 11th, 2021, CBIT performed a routine, fracture critical, and element level inspection of this structure in accordance with NBIS, FHWA, IDOT, and CDOT guidelines. The findings from this inspection indicate the following general ratings for this structure:

- Deck is in satisfactory condition (overall condition rating = 6)
- Superstructure is in poor condition (overall condition rating = 4)
- Substructure is in fair condition (overall condition rating = 5)

These overall condition ratings correspond to the rating terminology defined by both NBIS and IDOT guidelines. According to the Illinois Highway Information System Structure Information and Procedure Manual, the structural evaluation of a bridge is generally coded no higher than the lower of the deck’s overall condition rating, the superstructure’s overall condition rating, or the substructure’s overall condition rating. Consequently, this structure is in poor overall condition, which corresponds to an NBIS and IDOT general condition rating of 4.

### Condition Rating History

	2011	2013	2015	2017	2019
Deck	6	6	6	6	6
Superstructure	4	4	4	4	4
Substructure	5	5	5	5	5

### GENERAL STRUCTURE INFORMATION

Structure: 95th Street over the Calumet River

Structure No.: 016-6038

Bridge Description: The structure is a double-leaf, trunnion type bascule bridge. The structure has an overall length of approximately 343 feet, a roadway width of approximately 62 feet and an overall width of 87 feet.

Year Built: 1958, Reconstruction 1995

ADT: 2019 – 10,000 (5% Trucks)

Inspection Date/Duration: 8/11/21 - 4 inspectors @ 5 hrs. = 20 hrs. (movable span)

Temp./Weather Condition: 82°F / Partly Cloudy

<u>Required Inspections:</u>	<u>Type</u>	<u>Frequency</u>	<u>Previous Date</u>
	Routine	24 months	08/06/2019
	Element Level	24 months	08/06/2019
	Fracture Critical	12 months	08/31/2020
	Special	N/A	
	Underwater	60 months	06/04/2019

Bridge Status: The bridge is open to traffic with the following weight limit posting (see Photograph 1).

- Single Vehicle - 22 Tons
- Combination 3 or 4 axles - 29 Tons
- Combination 5 or more axles - 36 Tons

Additional Information: See Master Structure Report (S-107) at the end of this report for additional structure information.

Access & Equipment: The underside of the movable span was inspected from a 60 ft. manlift on a 110 ft. x 35 ft. barge. The remaining portions of the bridge were visually inspected from the ground and from a 15 ft. ladder where necessary. The bridge houses require the CDOT keys to access. Since this bridge opens for marine traffic upon request, maintaining radio contact with the operator while on site is required.

Traffic Control: None

Firm – Inspectors Present: Collins Engineers, Inc. – Joshua Simpson, Jacob Behnke, John  
(no initials) Cooke, and Jordan Hayes

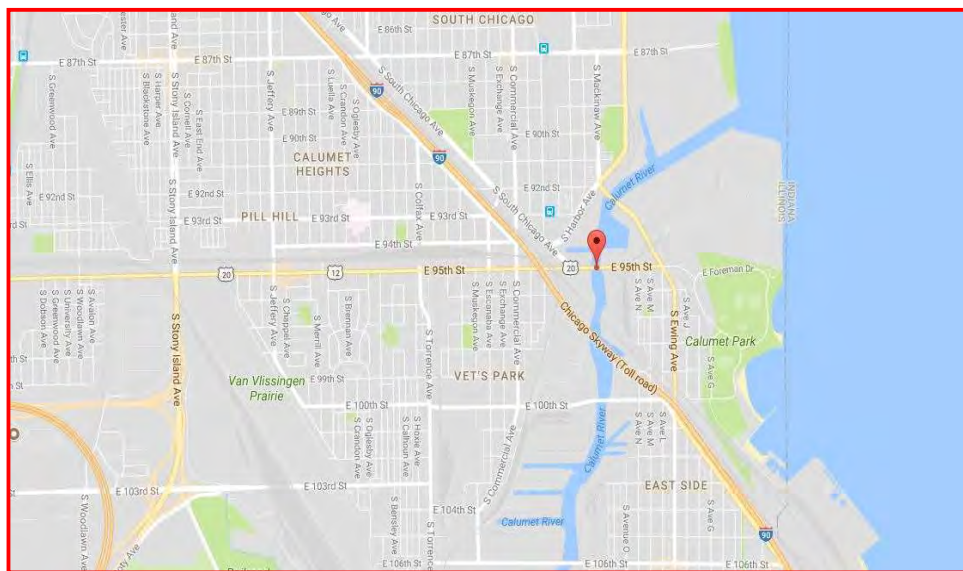
**Remarks:** The movable leaves of the bridge are operable.

**Terminology:** For the purpose of this report, the spans are numbered from east to west, and the stringers are numbered from north to south.

Section loss described in this report ranges as follows:

- Minor up to 10%
- Moderate > 10% up to 30%
- Heavy > 30%

**Location Map:**



### INVENTORY INFORMATION

- The bridge is open to traffic and is load posted (see Photograph 1).
- The weight limit signs are located east of the structure at the intersection of 95<sup>th</sup> Street and S Ewing Avenue, and west of the structure at the intersection of South Chicago Avenue and 95<sup>th</sup> Street.



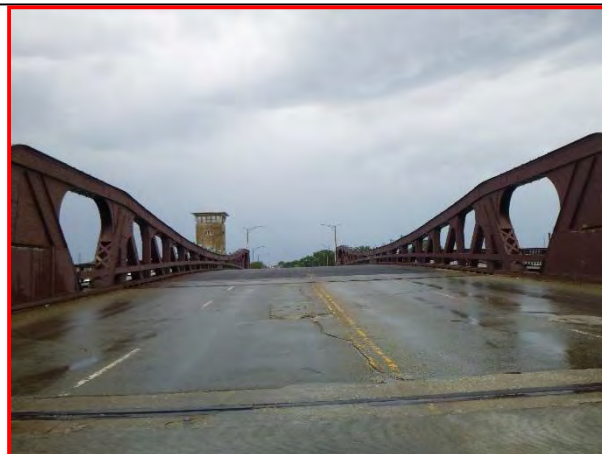
1. Weight limit posting sign, looking east.

#### I. DECK

- The deck is in satisfactory overall condition corresponding to a NBIS and IDOT condition rating of 6.

##### *Wearing Surface*

- In Span 3 the open steel grating wearing surface exhibits a few broken top bars and several welded plate repairs (see Photograph 3).



2. Overall top of deck, looking north.

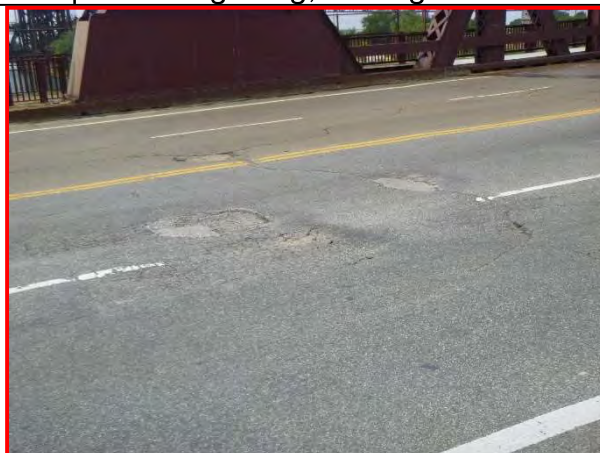


3. Steel plate repairs along the open steel grating, looking northeast.

- In Span 3, the concrete-filled portions of the grating near the rear breaks exhibit light scaling, minor concrete pop-outs, minor to moderate corrosion of the steel grating, and a few broken banding bars along the edges (see Photograph 4). Some areas have been covered utilizing welded plate repairs.
- The bituminous wearing surface in the fixed spans exhibits scattered wide diagonal cracks, rutting along the shoulders, and uneven surfaces as well as a few potholes and rough patching (see Photographs 5-7).
- The pavement markings are faded on Span 3 and visible elsewhere.



4. Span 3 interface of concrete filled and open steel grating, looking north.



5. Spans 1 and 2 wearing surface, looking southwest.



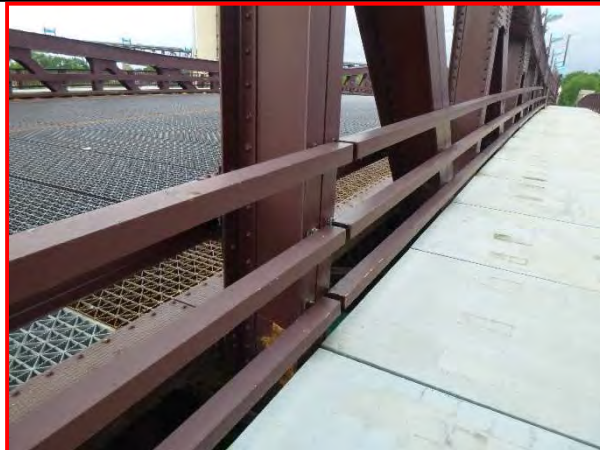
6. Span 2, south curb, looking west.



7. Span 4, north curb, looking east.

*Parapets/Bridge Railings*

- The railings in Span 3 exhibit isolated areas of moderate corrosion and scattered paint peeling/scrapes throughout (see Photograph 8).
- Span 3, there are steel railings field welded to the outside face of the vertical and diagonal truss members above the sidewalk.
- Sections of the fixed span pedestrian railings have been replaced (see Photograph 9).



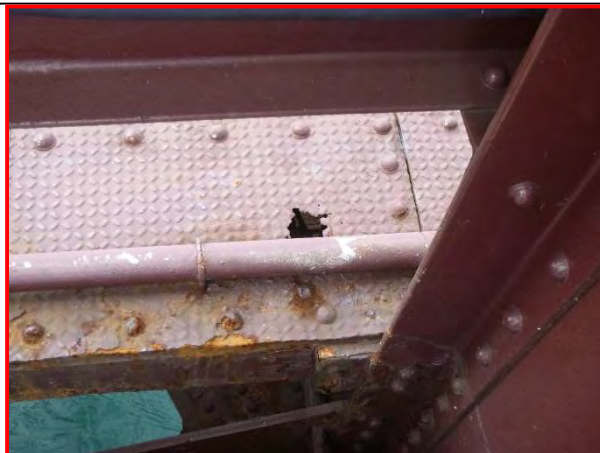
8. Typical pedestrian railing, looking southwest.



9. Fixed span pedestrian rail, looking northwest.

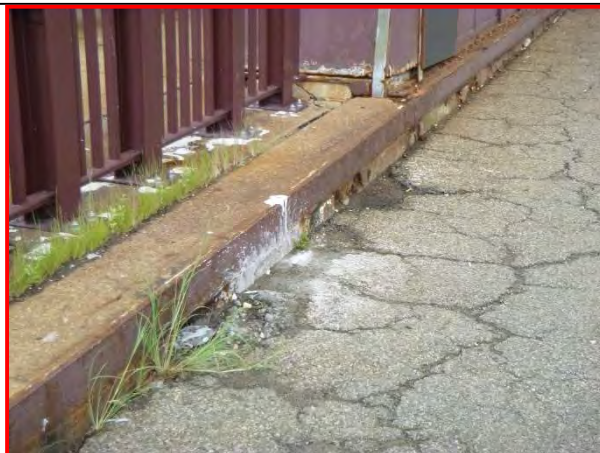
*Curbs*

- In the movable span, the steel curbs exhibit areas of minor deterioration and scattered corrosion holes in the cover plate (see Photograph 10).



10. North curb of the east leaf, looking south.

- In the fixed spans, the concrete along the steel curb plates typically exhibits minor spalling (see Photograph 11).



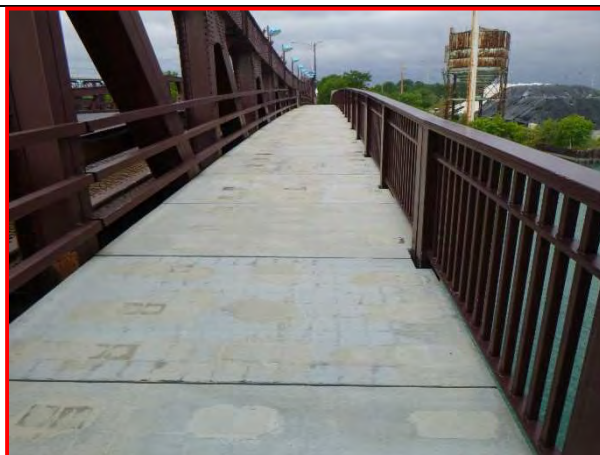
11. Spans 4 and 5 steel curb, looking northeast.

*Median*

- There is no median on the structure.

*Sidewalks*

- The Span 3 sidewalks have been replaced (see Photograph 12).
- The north sidewalk exhibits section loss and a bent steel curb with surrounding vegetation at the west expansion joint (see Photograph 13).
- The northwest approach sidewalk exhibits several spalls, and vegetation growth (see Photograph 14).



12. Typical Span 3 sidewalk, looking west.



13. North sidewalk at West Expansion Joint, looking down/south.



14. Northwest approach sidewalk, looking west.

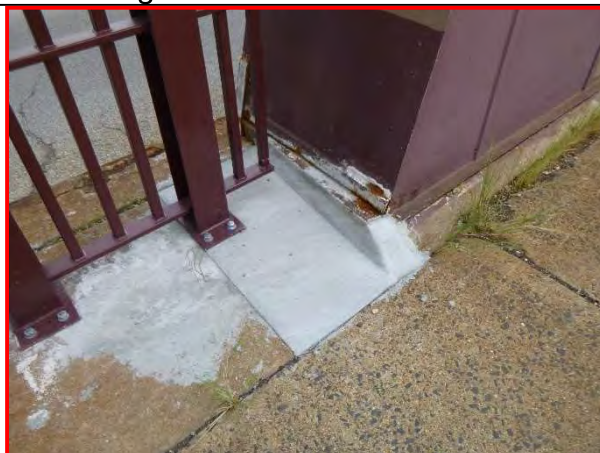
- The south sidewalk at the east expansion joint exhibits heavy spalling, with heaving up to 4” vertically (see Photograph 15).
- The south sidewalk in Spans 1 and 2 exhibits heaving (see Photograph 16).
- There is a concrete patch at the southwest corner of the truss. (see Photograph 17).
- The southwest approach sidewalk has been patched (see Photograph 18).
- The northeast approach sidewalk has a grate approximately 20’ from the east expansion joint that is bent in up to 2” (see Photograph 19).



15. South sidewalk at east expansion joint, looking east.



16. Spans 1 and 2 sidewalk, looking east.



17. Southwest corner of truss, looking northeast.



18. Southwest approach sidewalk, looking east.



19. Typical fixed span sidewalk, looking east.

*Drain System*

- There are no drains on the structure.

*Light Standards*

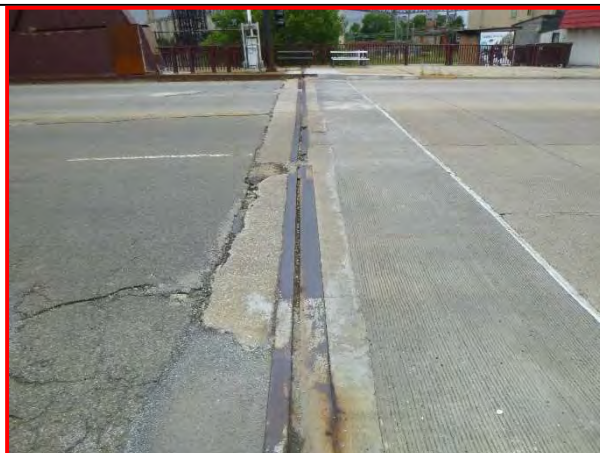
- The light standards exhibit moderate to heavy corrosion near the bases, corroded or missing anchor bolt covers, and missing hand hole covers with exposed wiring at the Southwest Light Standard and the Northwest Approach Light Standard (see Photograph 20).



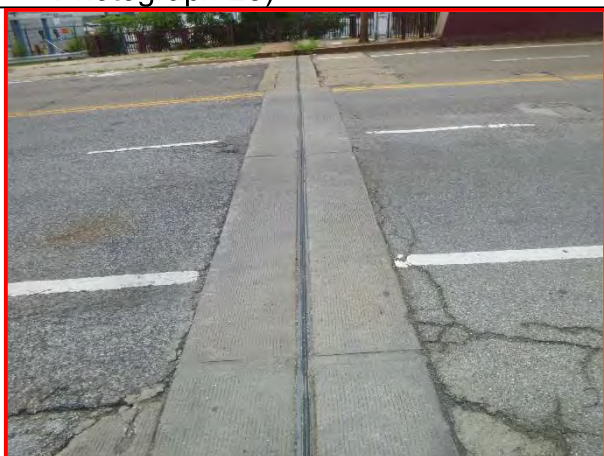
20. Typical light standard base, looking east.

*Expansion Joints / Relief Joints / Breaks*

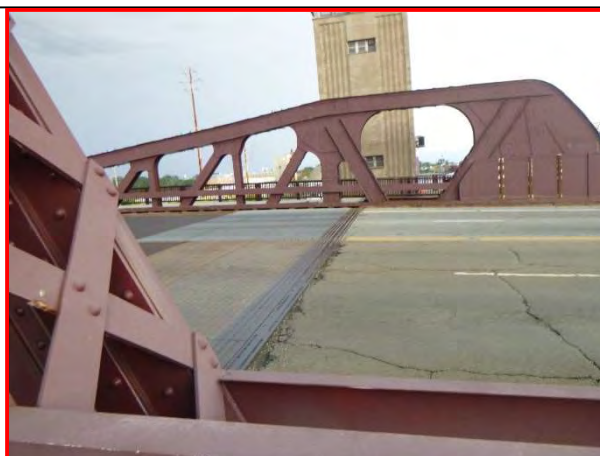
- The West Expansion Joint exhibits missing sections of armor plate in three locations (which have been filled in with bituminous material), debris accumulation, and wide cracks and spalling along the concrete blockout (see Photograph 21).
- The East Expansion Joint exhibits minor debris accumulation and the gland is bulging along  $\frac{3}{4}$  of the length (see Photograph 22).
- Both expansion joints at the sidewalks exhibit heavy corrosion and deteriorated sections of gland with vegetation growth.
- The center and rear breaks have a few areas of minor corrosion and there are polished sections of the steel plates along the wheel lines (see Photograph 23).



21. West joint condition, looking north.



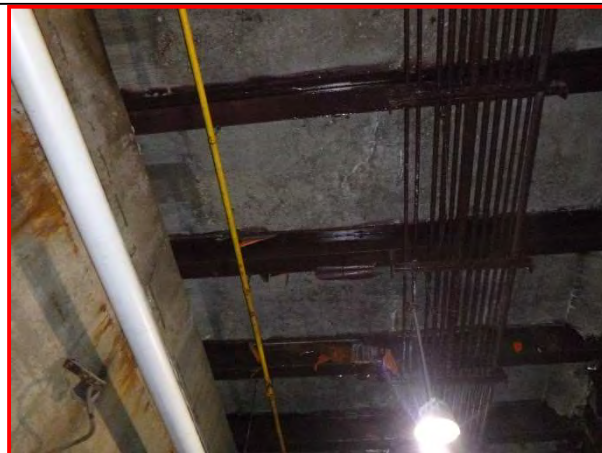
22. East expansion joint, looking south.



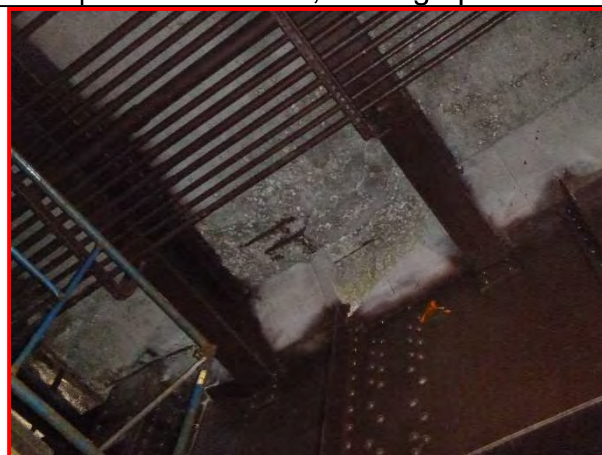
23. Span 3 east rear break, looking north.

*Deck Soffit*

- In the movable span, the steel grating with concrete-filled portions exhibits minor to moderate corrosion.
- A deck patch is located on the west fixed span between Stringers 2 and 3 from the north.
- In the fixed span, the deck soffit exhibits hairline to narrow leaching cracks, scattered minor concrete scaling, as well as isolated spalls with exposed and corroded reinforcement primarily in Spans 4 and 5 (see Photograph 24 and 25).



24. Span 1 deck soffit, looking up.



25. Span 5 deck soffit, looking up and northeast.

*Approaches*

- The concrete and bituminous approaches exhibit scattered hairline longitudinal cracks, with wide cracks in the East Approach (see Photograph 26).



26. East approach, looking east.

*Approach Guardrails*

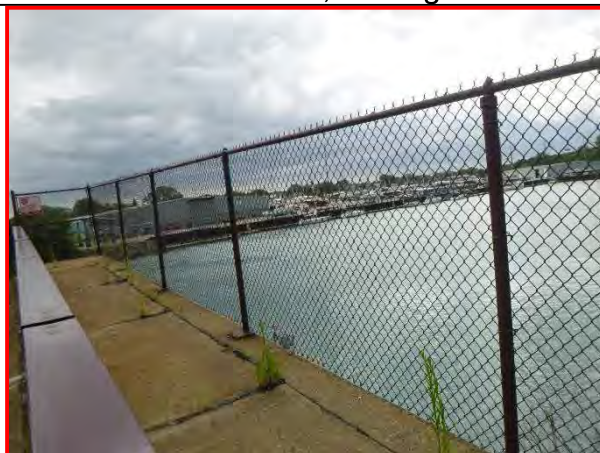
- There are no guardrails on the structure; however, temporary concrete barriers are in place along the Northeast Approach Sidewalk (see Photograph 27).



27. Temporary concrete barriers along northeast sidewalk, looking east.

*Fences*

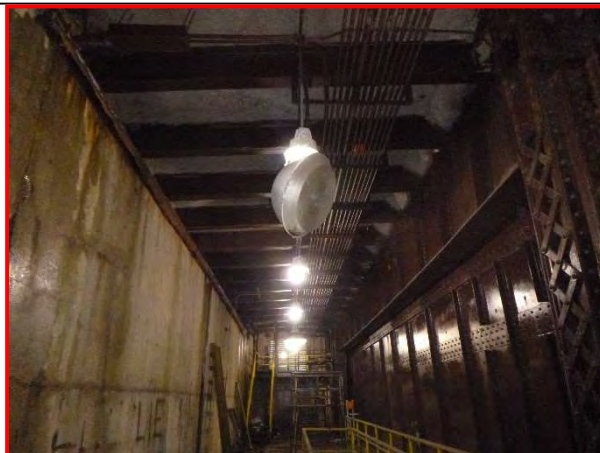
- There is chain link fencing behind the bridge railing along all four corners of the bridge that exhibits corrosion (see Photograph 28).



28. Fence at the southwest corner, looking southeast.

*Utilities*

- There are utilities and light fixtures attached to the deck soffit in the fixed spans (see Photograph 29).
- There is an overhead utility running along the southwest light standard into the west approach.



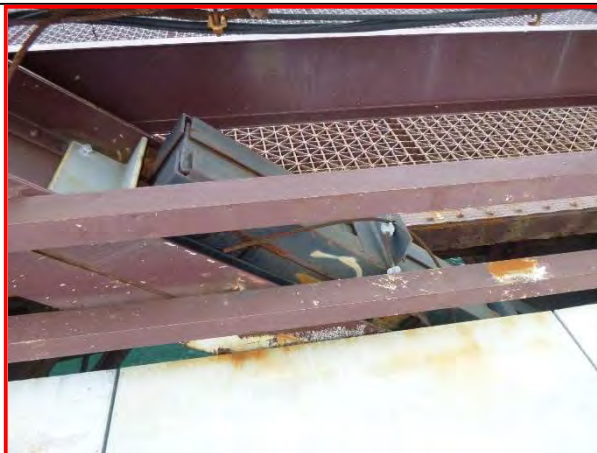
29. Span 5 utilities, looking north.

*Signage*

- The bridge signs and “No Wake” signs fastened to the bridge railings in Span 3 have no noted deficiencies, and there is a “No Parking” sign with minor graffiti fastened to the east end of the south bridge railing. The load limit sign exhibits no noted deficiencies.

*Other*

- The center truss has counterweight boxes attached to the truss (see Photograph 30).
- The life preservers are in place at the northeast and northwest corners (See Photograph 31).
- The traffic and pedestrian barriers are in place in Spans 1 and 5. The northeast traffic Span 1 barrier exhibits repairs made with wood and tape (see Photograph 32).



30. Span 3 weight blocks, looking south.



31. Life preserver at northwest corner, looking north.



32. Span 1 northeast traffic barrier, looking southwest.

**II. SUPERSTRUCTURE**

- The superstructure is in poor overall condition which corresponds to a NBIS and IDOT condition rating of 4 (Note – this condition rating is governed by the Fracture Critical Rating).



33. Overall underside of the movable superstructure, looking northwest.

*Bearings*

- The abutment bearings exhibit several cracked castings, several missing anchor bolts, and moderate to heavy section loss on the anchor bolt, with up to 3 inches of misalignment to the West Abutment bearings (see Photograph 34).
- The grout pads at the East Abutment bearings are heavily cracked and spalled (see Photograph 35).
- The trunnion bearings in the fixed spans exhibit minor to moderate corrosion with minor section loss (see Photograph 36).
- The live load bearings exhibit moderate corrosion and moderate to heavy section loss and minor debris accumulation. The anchor bolts have moderate section loss (see Photograph 37).



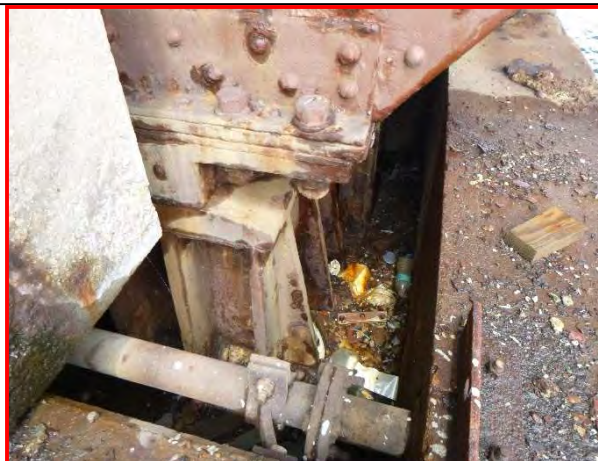
**34.** West Abutment bearings, looking southwest.



**35.** East Abutment bearings, looking east.



**36.** Southeast trunnion bearing, looking northwest.



**37.** Southeast live load bearing, looking north.

- West Live Load Bearings, the top shoes are out of alignment with the bottom castings up to 1/2 inch to the south.
- The secondary live load bearings on the anchor columns exhibit pack rust between the bumper stiffener plates, and there are no gaps between the steel bumpers and the anchor shoes (see Photograph 38).
- The longitudinal girder bearings on the river piers exhibit moderate corrosion (see Photograph 39).



38. Northeast secondary live load bearing, looking west.



39. Southwest longitudinal girder bearing, looking south.

#### *Stringers*

- In the fixed span, the stringers exhibit minor section loss near the abutments and minor section loss with corrosion holes near the rear breaks (see Photograph 40).



40. Stringer ends at east rear break, looking north.

- The sidewalk stringers in the fixed spans exhibit moderate to heavy section loss with a few corrosion holes.
- In the south end of Span 4, a sidewalk stringer exhibits section loss and a 24" by 3" corrosion hole near the connection (see Photograph 41).
- At the west rear break, Stringer ends 3-5 and 8-12 of the moveable span have bent flanges, and minor section loss of the bottom flange and the lower 3-inches of the web (see Photograph 42).
- In the movable span, the roadway stringers typically exhibit minor to moderate corrosion, widespread corrosion holes (up to 4" long by 5" high) along the web, and paint peeling and flaking (see Photograph 43).
- In the movable span, there are varying degrees of corrosion, section loss and corrosion holes along the web and stringer end connections, as well as various stringer and connection repairs. The cantilever stringers supporting the center break exhibit moderate corrosion and minor section loss. The sidewalk stringers in the movable span exhibit minor to moderate corrosion with section loss and scattered corrosion holes. A few of the sidewalk stringers are out of plumb. See Exhibit 1 located at the end of the report for deficiencies and repairs in Movable Span Stringers (see Photographs 44 to 50).



41. Span 4 south sidewalk stringer, looking north.



42. Section loss along a stringer in Span 3, looking southeast.



43. Panel 8 stringers, looking north.



44. Panel 7, Stringer 1, looking south.



45. Panel 8, Stringer 1, looking south.



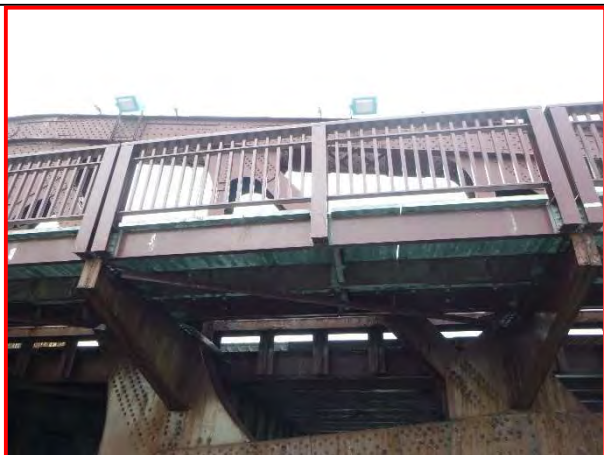
46. Panel 9, Stringer 9, looking southeast.



47. Panel 9, Stringer 4, looking north.



48. Panel 12, Stringer 6, looking southwest.



49. Panel 13 replaced sidewalk stringer, looking north.



50. Span 2 south sidewalk stringer, looking northeast.

**Girders**

- The trunnion girders and trunnion cross girders exhibit localized areas of moderate to heavy corrosion with minor to moderate section loss and paint peeling (see Photographs 51 and 52).
- The west trunnion cross girder is rotated up to 3 degrees out of plumb at both ends.
- Localized areas of vertical stiffener corrosion and minor section loss are present at the trunnion girder ends near the river piers (see Photograph 53).



**51.** Southeast trunnion girder, looking south.



**52.** Southeast trunnion girder, looking north.



**53.** Vertical stiffeners of the southeast trunnion girder, looking east.

**Floorbeams**

- The fixed span floorbeams exhibit isolated areas of minor corrosion and large areas of paint peeling.
- The bottom flange of the west anchor column floorbeam (FB 21W) is bowed and is displaced towards the river up to 4" at midspan (see Photograph 54).



**54.** West anchor column floorbeam (FB 21W), looking south.

- The top 20” of the web of the west anchor column floorbeam (FB 21W) is bent in up to 2.25” directly below the S1 and S12 connections. Additionally, the top flange is bent and severed (see Photograph 55).
- Floorbeam 19W exhibits cracks approximately 1.5” long starting from the copes in both top corners. Additionally, the web is out of plumb up to approximately 4.5 degrees.
- In the movable span, the floorbeams typically exhibit minor to moderate corrosion with minor to moderate section loss, scattered corrosion holes of varying size (up to 4” x 4”) on the webs, moderate to heavy rivet head loss on the bottom flange, and corrosion holes in the vertical web stiffeners near the bottom flange (see Photograph 56).
- Floorbeams in the movable span typically exhibit minor pitting (see Photograph 57).
- Floorbeam 0W exhibits heavy paint peeling in the web (see Photograph 58).



55. South end of floorbeam 21W, looking northeast.



56. Typical moveable span floorbeam, looking southeast.



57. Typical floorbeam pitting, looking east.



58. West end of FB 0W, looking east.

- Several floorbeams have corrosion holes near the stringer and lateral brace connections, as well as near truss connections (see Photographs 59).



59. Typical corrosion hole near stringer connection, looking northwest.

*Lateral Bracing*

- In the movable span, the lateral bracing exhibits heavy corrosion, corrosion holes, several broken lacing bars, and a few locations of bent flanges (see Photographs 60-62).
- Several sidewalk lateral bracing connection plates have corrosion holes, and some lateral bracing members are also sagging.
- Span 2 and 4, the lattice trusses and connections exhibit moderate to heavy corrosion and scattered corrosion holes.



60. Panel 13 Lateral bracing stiffener plate, looking west.



61. Panel 1 lateral bracing, looking east.



62. Panel 1 lateral bracing, looking east.

- In the movable span, the lateral bracing connection plates exhibit heavy corrosion with large corrosion holes (see Photograph 63).



**63.** Floorbeam 2W Lateral bracing connection, looking west.

*Trusses*

- The bascule trusses exhibit corrosion with scattered areas of minor to moderate section loss, pack rust, and scattered locations of bent members (see Photograph 64).
- In the moveable span, bottom chord members at the northeast and northwest supports exhibit bent flanges (see Photographs 65-66).
- Near the rear end of the trusses over the counterweight pits, the truss bottom chords exhibit minor to moderate corrosion along the bottom flanges with moderate rivet head loss.
- Pack rust is typically present along the bottom chord and fill plate interface.



**64.** Panel 7, south bottom chord member, looking north.



**65.** Bottom chord member at northwest support, looking west.



**66.** Bottom chord member at northeast support, looking east.

- The truss diagonals and verticals typically exhibit minor corrosion and minor paint peeling along the deck interface (see Photograph 67).

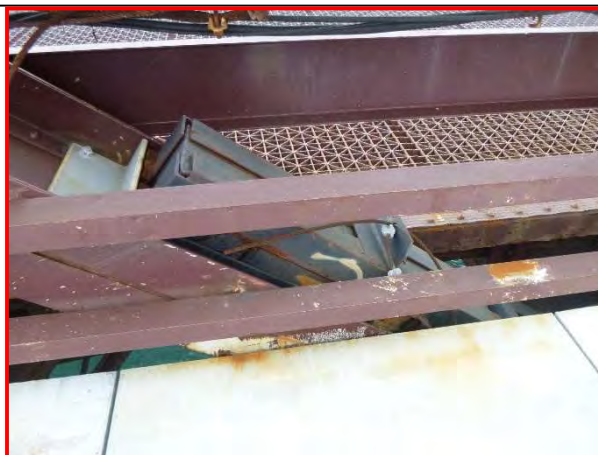


**67.** Bottom chord member at northwest support, looking west.

- Truss top chord members typically exhibit moderate pitting, corrosion, and paint peeling (see Photograph 68).
- Counterweight blocks are located on the north and south top chords of the east leaf (see Photograph 69).



**68.** Top chord of south truss, looking northeast.



**69.** Counterweight at north truss, looking south.

*Counterweights*

- Concrete spalls are present throughout the front face of the counterweight box (see Photograph 70).
- The counterweights exhibit light corrosion on the steel framing plates (see Photograph 71).



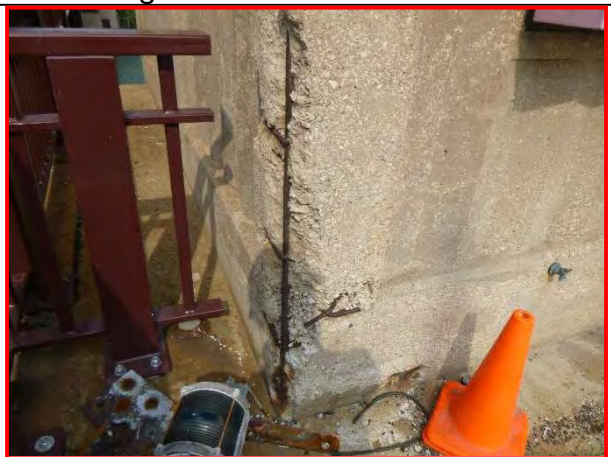
70. East counterweight box, looking east.



71. Bottom of west counterweight box, looking north.

*Bridge Houses*

- At the East Bridge House, the exterior faces of the enclosure walls above the deck exhibit several spalls with exposed reinforcement (see Photograph 72).
- At the East Bridge House, the exterior face of the north enclosure wall has a large spall along the east construction joint.
- At the East Bridge House, there is a storage room in place along the south half of the East Abutment.
- At the East Bridge House, several of the steps leading down to the East Counterweight Pit are spalled and have cracks.



72. Northeast bridge house, looking west.

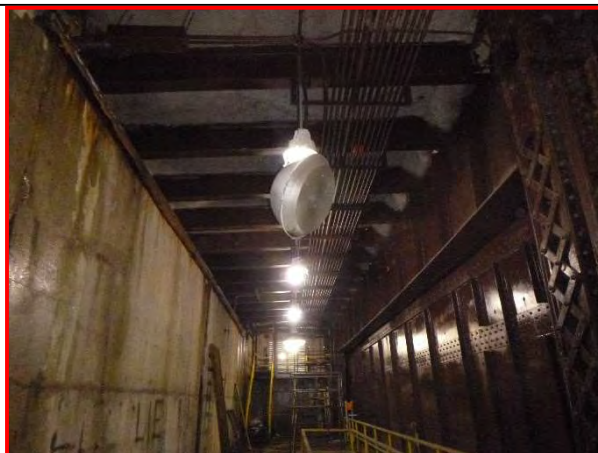
- The support towers below the exterior trunnion bearings in both bridge houses exhibit large areas of paint peeling.
- At the West Bridge House, the exterior face of the south enclosure wall above the deck exhibits a spall with exposed reinforcement.
- At the West Bridge House, the steel staircase leading down to the bridge house floor has been repaired.
- West Bridge House, the railing along the steps leading down to the West Counterweight Pit have a fully corroded out connection at the bottom and several of the steps have loose tread plates (see Photograph 73).



73. Railing in west bridge house, looking southeast.

*Utilities*

- There are several utilities attached to the bottom flanges of the stringers in the fixed spans. Several of the conduits exhibit heavy corrosion (see Photograph 74).
- In the movable span there are several utilities attached to truss members and along the center break floorbeams.
- In the movable span, the light fixtures attached to the superstructure have no noted deficiencies.



74. Span 5 utilities looking north.

*Other*

- The catwalks below the center break exhibit moderate pitting, rust packing, and some corrosion holes (see Photograph 75).
- The red and green navigational lights are in place and functioning; Red navigation light at the southeast catwalk has a cracked housing.



75. Catwalk on south truss, looking east.

- The catwalks in the west pit exhibit heavy section loss (see Photograph 76).



76. West pit catwalk steel plating, looking north.

### III. SUBSTRUCTURE

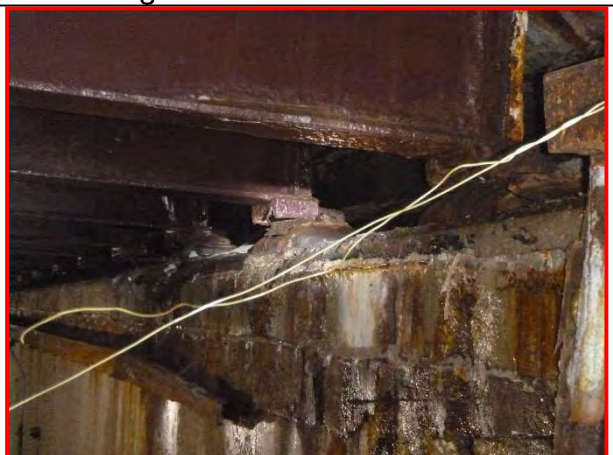
- The substructure is in fair overall condition corresponding to a NBIS and IDOT condition rating of 5.

#### *Abutments/Wingwalls*




- At the West Abutment, the bearing seats and abutment back wall exhibit delaminated and spalled areas throughout, debris accumulation, signs of leakage, as well as spalled and cracked grout pads (see Photograph 77).
- The East and West Abutment stems exhibit scattered hairline to narrow cracks and a few spalls (some with exposed and corroded reinforcement).
- At the West Abutment, the bearing seats and abutment backwall exhibit heavy deterioration with delaminated and spalled areas and some debris accumulation (see Photograph 78).



77. West Abutment stem and bearing seat, looking southwest.



78. West Abutment bearing seat, looking southwest.

<p><i>Piers</i></p> <ul style="list-style-type: none"><li>• The East and West River Piers both exhibit large spalls with penetrations up to 1' deep. Concrete cracking and leaching with some widespread leakage (see Photograph 79).</li><li>• The east face of the East Pier exhibits several wide vertical cracks with associated spalls up to 8" depth and heavy scaling below the north live load bearing.</li><li>• The Steel Pier Caps exhibit isolated areas of minor corrosion with paint peeling.</li></ul>	 <p>79. East face of river pier wall, looking west.</p>
<p><i>Columns</i></p> <ul style="list-style-type: none"><li>• The batten plates near the base of the anchor columns are heavily corroded with corrosion holes (see Photograph 80).</li><li>• The trunnion girder columns exhibit areas of minor section loss with corrosion holes in the lacing bars, as well as heavy rivet head loss.</li><li>• The sidewalk columns exhibit minor section loss near the bases. The grout pads of the sidewalk columns are cracked and spalled.</li></ul>	 <p>80. West anchor column and trunnion column, looking southeast.</p>
<p><i>Counterweight Pits</i></p> <ul style="list-style-type: none"><li>• The counterweight pit side walls exhibit narrow to medium vertical and horizontal cracks with leaching, as well as several large spalls with exposed and corroded reinforcement (see Photograph 81).</li><li>• There is cracked and spalled concrete below the machinery bearing plates at midspan of the side walls.</li><li>• There is debris and steel counterweight blocks within the pits.</li><li>• During the time of inspection there was no standing water in the counterweight pits.</li></ul>	 <p>81. Overall view of east pit, looking down/northeast.</p>

*Fender System*

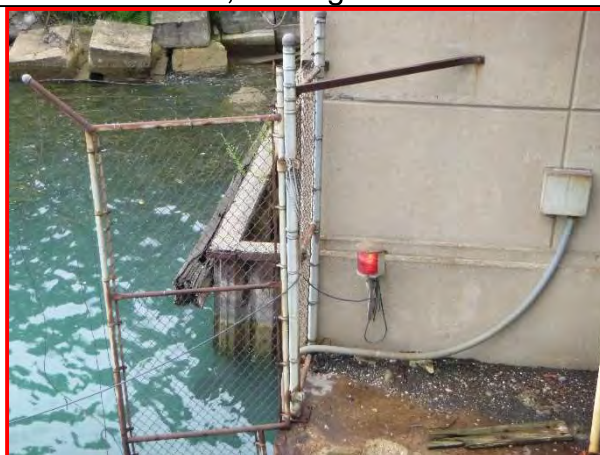
- The fender system is in place with damaged sections on the East and West River Piers (see Photograph 82).
- The dolphins located in the northwest, southwest, and southeast corners of the bridge are in place and functioning, and the timber piles exhibit deterioration.



82. West fender, looking west.

*Other*

- The red navigation lights attached to the substructure are in place and functioning. The south navigation light on the West River Pier has a broken housing.



83. Southwest navigation light, looking west.

**IV. STEEL PROTECTIVE COAT SYSTEM**

- On a scale of 5 to 1 (5 being new; 1 being failed), the paint system for this structure is rated a 2 (poor).

*Superstructure*

- The paint system has extensive deterioration along the movable span stringers and floorbeams (see Photograph 84).



84. Typical moveable span paint system, looking north.

- The fixed span stringers and floorbeams have scattered large areas of paint system failure (see Photograph 85).



85. Typical fixed span paint system, looking southeast.

*Substructure*

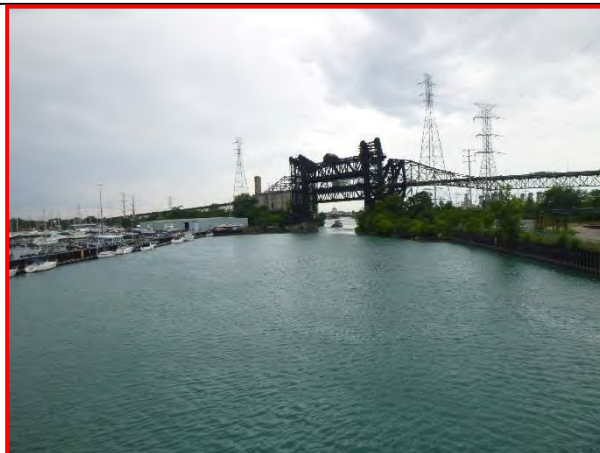
- The paint system on the anchor columns and trunnion girder columns have minor to moderate deterioration at the bases (see Photograph 86).



86. Typical substructure paint system, looking southeast.

**V. CHANNEL**

- The channel is in satisfactory condition which corresponds to a NBIS and IDOT general condition rating of 6.
- The west bank adjacent to the structure is beginning to slump.



87. Downstream channel, looking south.



88. Upstream channel, looking north.

**VI. UNDERWATER INVESTIGATION**

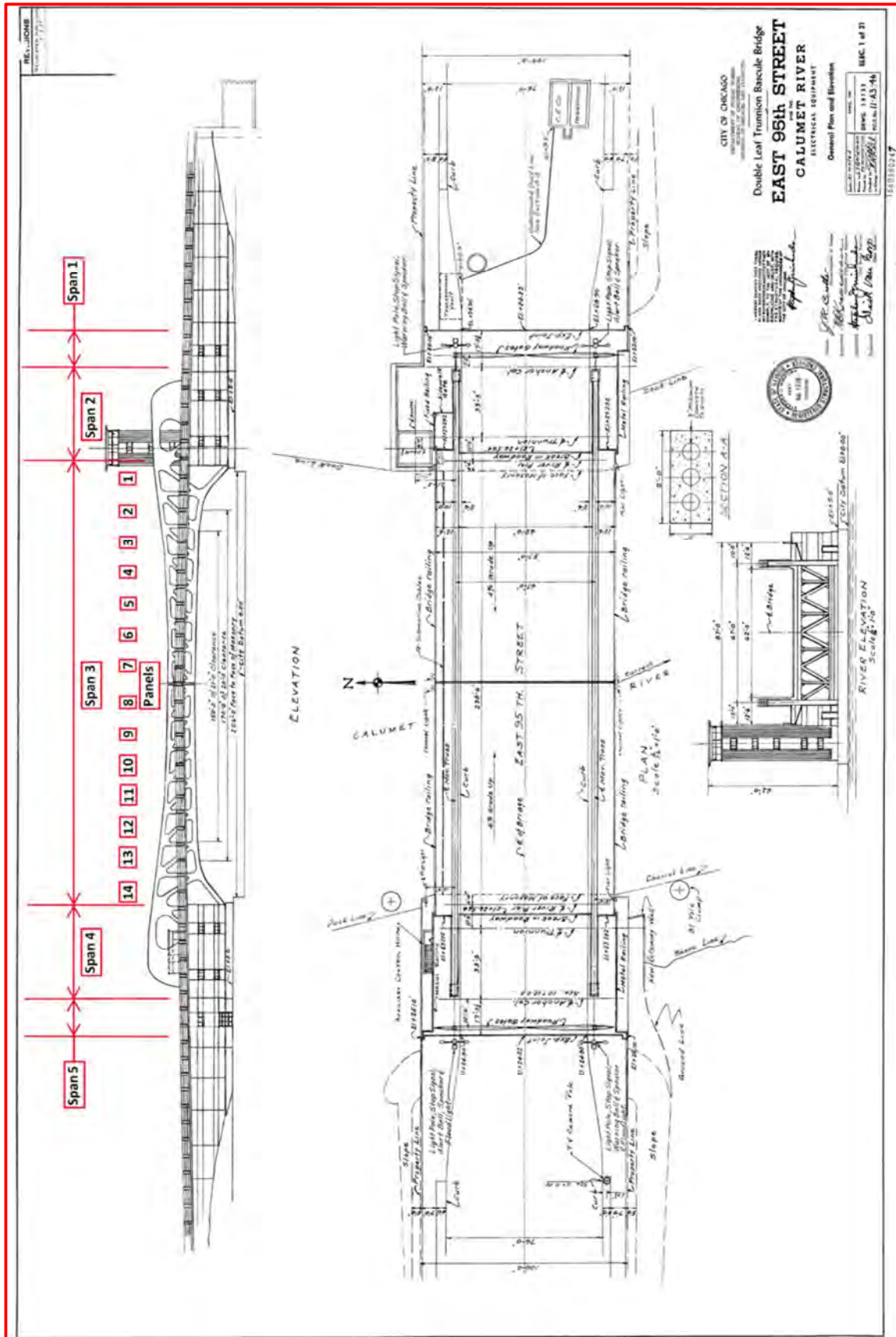
- An underwater investigation was conducted by Collins Engineers, Inc. on June 4, 2019. At the time of inspection, the affected elements were rated fair and given a NBIS and IDOT general condition rating of 5. Please refer to the underwater inspection report for specific inspection findings regarding the underwater bridge elements.

**VII. MECHANICAL INSPECTION**

- The mechanical inspection was conducted by HNTB Corporation on October 2016. Please refer to the mechanical bridge inspection report for specific inspection findings regarding the mechanical bridge elements.

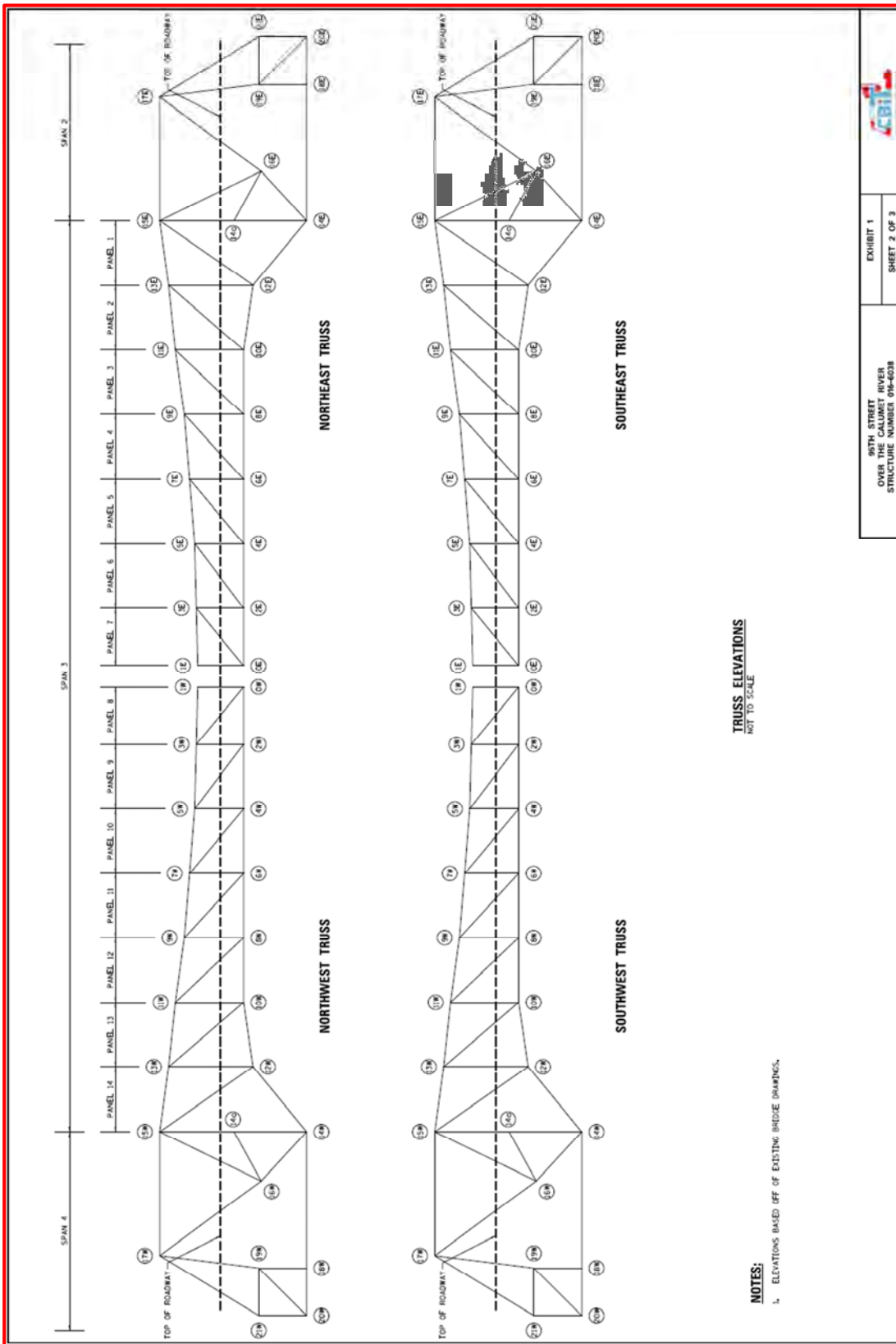
**VIII. ELECTRICAL INSPECTION**

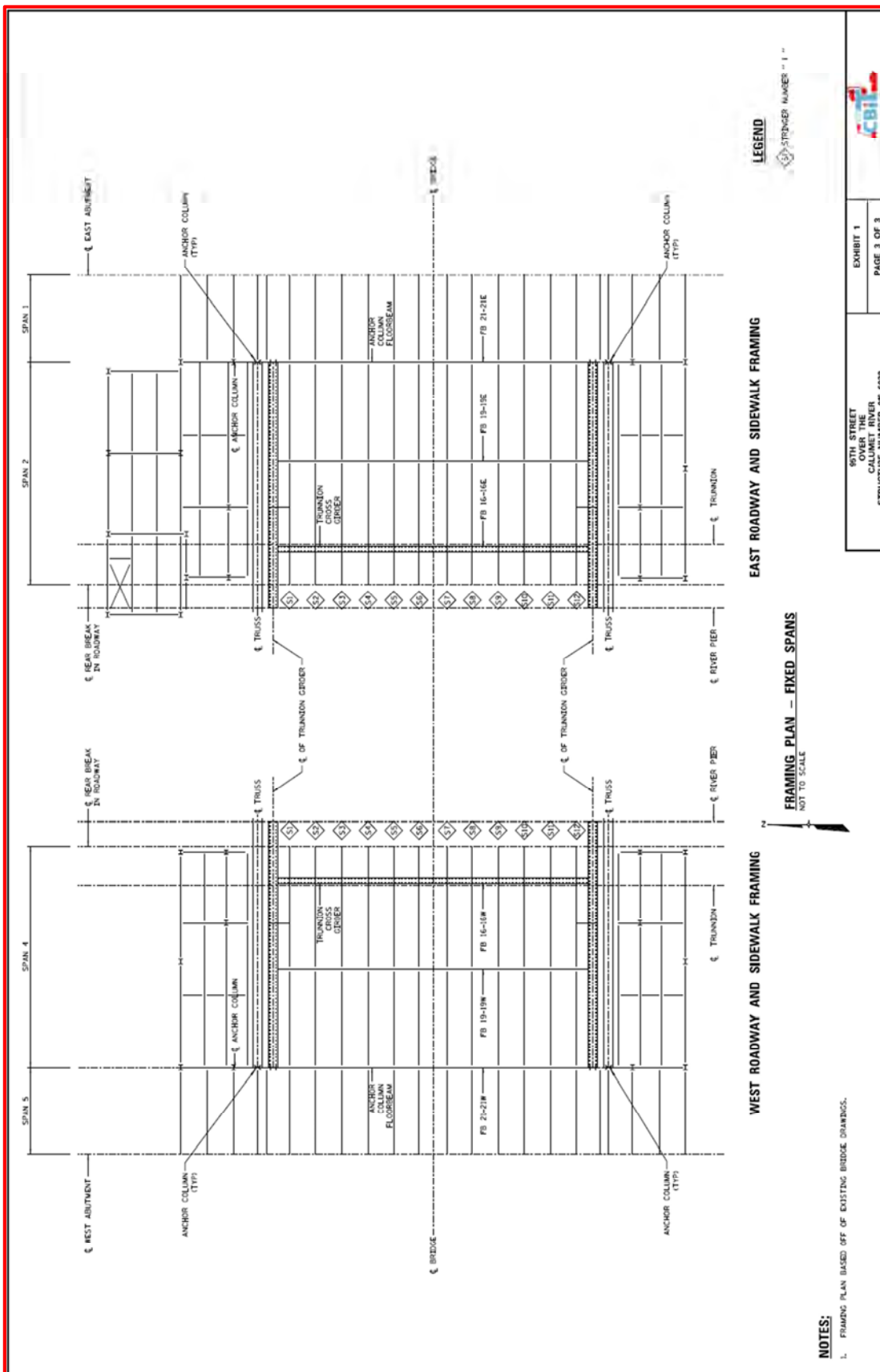
- The electrical inspection was conducted by HNTB Corporation on October 2016. Please refer to the electrical bridge inspection report for specific inspection findings regarding the electrical bridge elements.



General Plan and Elevation







LEGEND

STRENGTH MEMBER - 1"

EAST ROADWAY AND SIDEWALK FRAMING

WEST ROADWAY AND SIDEWALK FRAMING

FRAMING PLAN - FIXED SPANS  
 NOT TO SCALE

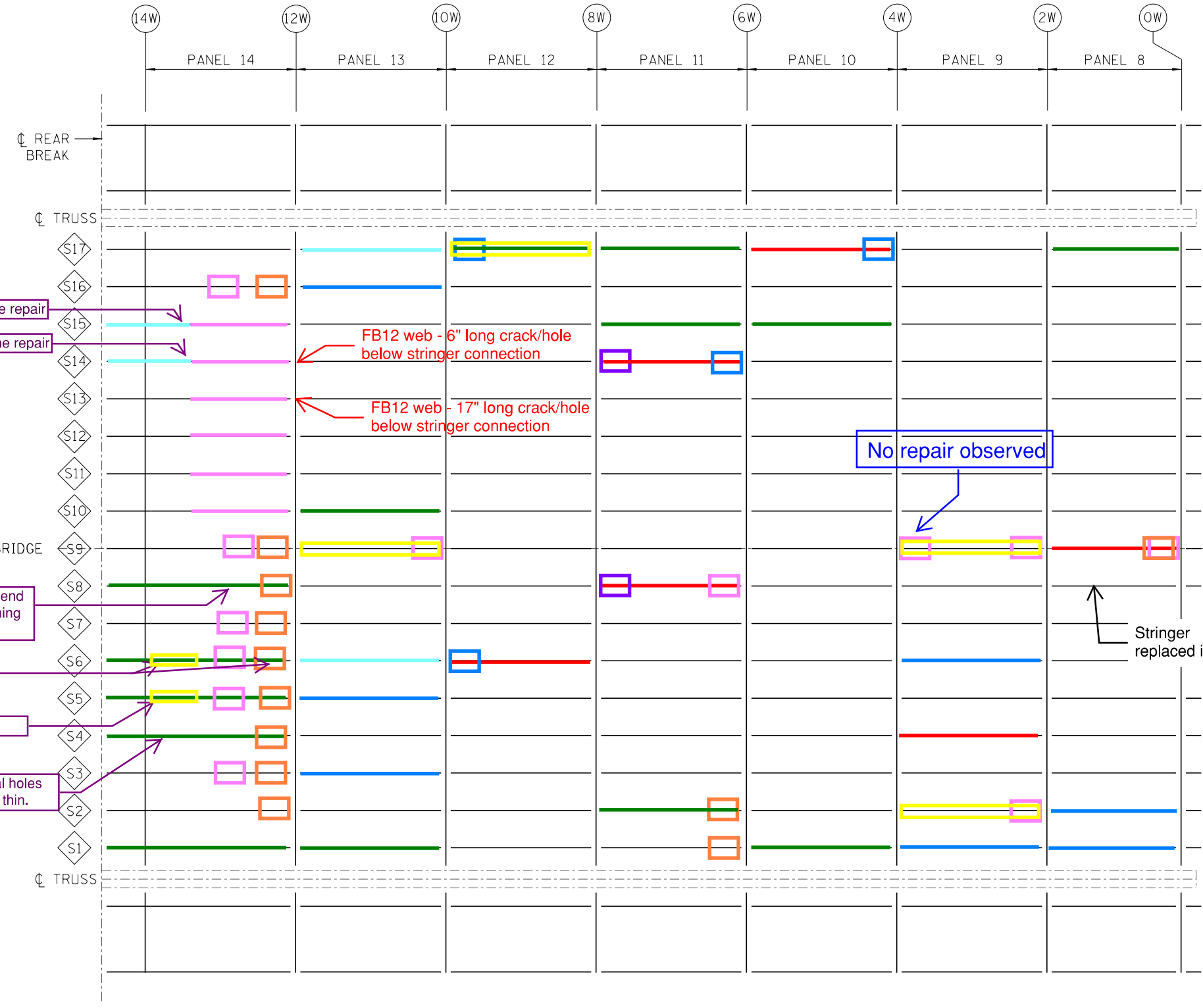
NOTES:

- FRAMING PLAN BASED OFF OF EXISTING BRIDGE DRAWINGS.

95TH STREET  
 OVER THE  
 CALUMET RIVER  
 STRUCTURE NUMBER 016-6038

EXHIBIT 1  
 PAGE 3 OF 3





- High Priority Defect (10 total)
- High/Medium Priority Defect (9 total)
- Medium Priority Defect (21 total)
- Medium/Low Priority Defect (7 total)
- Bolted Splice with Partial Length Replacement - 16 locations
- Plated Repair 2020 (Collins plans) - 5 locations
- Plated Repair 2019 (Emergency plans) - 3 locations
- Miscellaneous bolted repair - 14 locations
- Miscellaneous welded repair - 17 locations
- Moved up to High/Medium priority due to existing bolted repair - 7 locations

**Notes:**  
 1. All of the above are based on 2019 & 2020 Routine and FC inspections, not in-depth stringer inspections.  
 2. Low priorities not included in this exhibit.

Holes in the bottom of the web under the east end of concrete fill. (Middle portion) (3' from beginning to end)

2' east of FB 14-14 and east end has holes

Near FB 14-14, several holes

Near FB 14-14, there are several holes in the b/ half of web. B/ flange is thin.

FB12 web - 6" long crack/hole below stringer connection

FB12 web - 17" long crack/hole below stringer connection

No repair observed

Stringer replaced in 2020

**WEST LEAF**

**NOTES:**

1. FRAMING PLAN BASED OFF OF EXISTING BRIDGE DRAWINGS.



**FRAMING PLAN – MOVABLE SPAN**  
 NOT TO SCALE

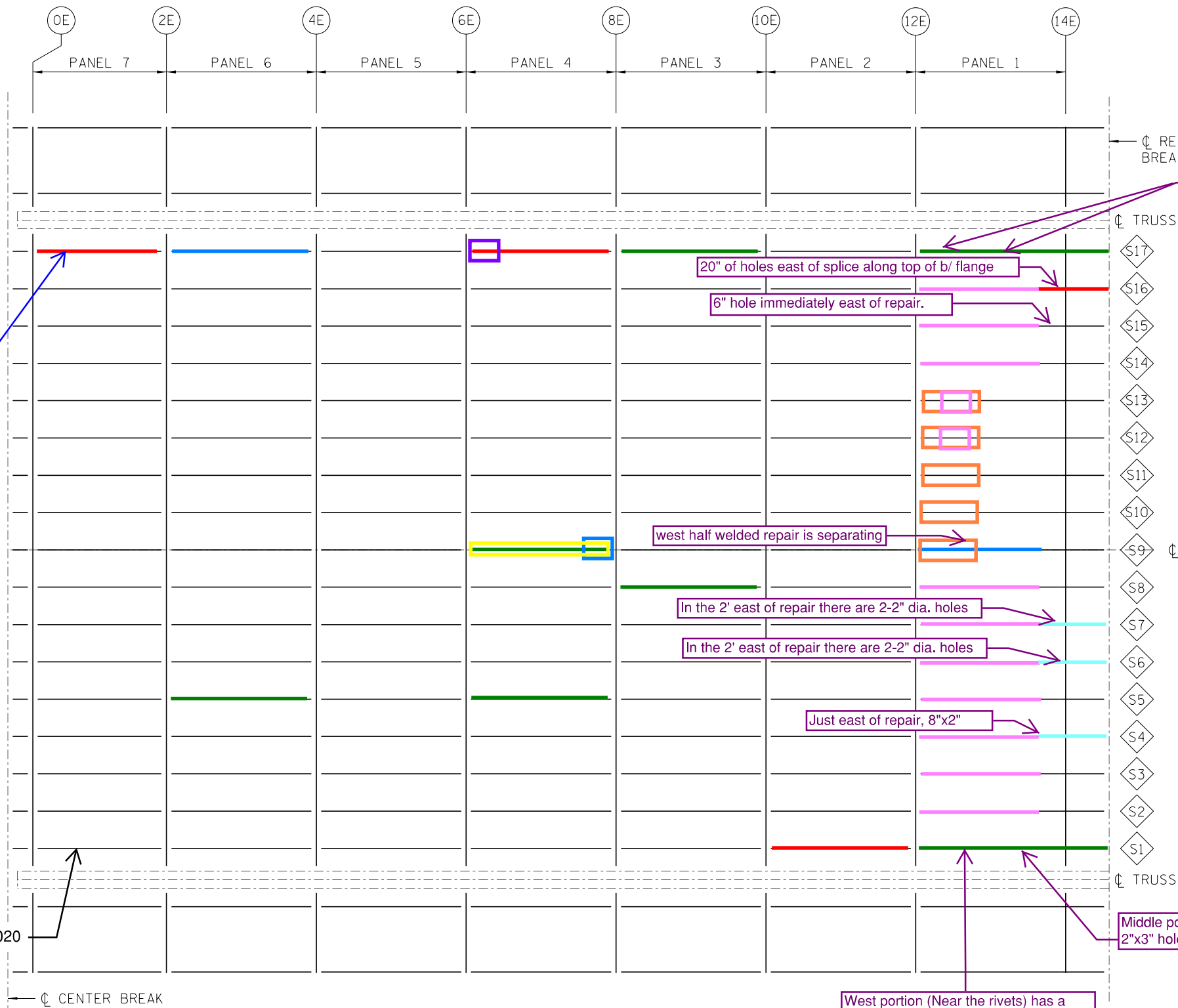
**LEGEND**

- OE PANEL POINT DESIGNATION
- S1 STRINGER NUMBER " 1 "
- CORROSION HOLE



Approx. 3" dia. hole through web

Stringer replaced in 2020



2" dia. hole just east of west rivets. 1" dia. hole just east of the middle rivets

20" of holes east of splice along top of b/ flange

6" hole immediately east of repair.

west half welded repair is separating

In the 2' east of repair there are 2-2" dia. holes

In the 2' east of repair there are 2-2" dia. holes

Just east of repair, 8"x2"

Middle portion (Near the rivets) has a 2"x3" hole

West portion (Near the rivets) has a 6"x3" hole

- High Priority Defect (10 total)
- High/Medium Priority Defect (9 total)
- Medium Priority Defect (21 total)
- Medium/Low Priority Defect (7 total)
- Bolted Splice with Partial Length Replacement - 16 locations
- Plated Repair 2020 (Collins plans) - 5 locations
- Plated Repair 2019 (Emergency plans) - 3 locations
- Miscellaneous bolted repair - 14 locations
- Miscellaneous welded repair - 17 locations
- Moved up to High/Medium priority due to existing bolted repair - 7 locations

**Notes:**  
 1. All of the above are based on 2019 & 2020 Routine and FC inspections, not in-depth stringer inspections.  
 2. Low priorities not included in this exhibit.

**NOTES:**

1. FRAMING PLAN BASED OFF OF EXISTING BRIDGE DRAWINGS.

**EAST LEAF**

**FRAMING PLAN – MOVABLE SPAN**  
 NOT TO SCALE



**LEGEND**

- PANEL POINT DESIGNATION
- ◇ STRINGER NUMBER " i "
- CORROSION HOLE





SN: 016-6038	District: 1	Spans: 1	Appr. Spans: 4	Skew: 0	ADT: 10000	Truck Pct: 5
ADT Un:	Maint. Co: COOK	Twsp: HYDE PARK (CHICAGO)	Status: OPEN - LOAD POSTED			
Facility Carried: FAP-29 (95TH ST)	Feature Crossed: Calumet River					
Location 3258 E.95TH STREET	Municipality: CHICAGO	Team/Sub: /	Insp/Rte:			
Bridge Name: 95TH STREET BRIDGE	Material & Type: STEEL/MOVEABLE - BASCULE					
Insp. Intervals Routine: 24	Fracture Critical: 12	Underwater: 60	Special: N/A	Element Level: 24		

90 – Inspection Date: 08 / 11 / 21	90C – Temp. (°F) 82	90B1 – In-Depth	<input type="checkbox"/>
Is Delinquent: <input type="checkbox"/>	Reason:		
90A – Agency Program Manager: Jurca, V.	90A3 – Consultant Program Manager: Bendok, M.		
90A1 – Team Leader: Simpson, J.	90A2 – Inspector: Behnke, J.		

**90B – Inspection Remarks:**

Previous Inspection	Joint Openings (In.) W. Abut., 1.5"; E. Abut., 1 3/8" (measured @ N. shoulder) Deck, the open steel grating has several welded repair plates throughout and the conc. filled grid has wide spread spalls and scaling along the surface and soffit. There a few through spalls in the sidewalk in Spans 4-5 and the movable span sidewalk has several heaved up sections up to the bottom of the steel railing. Superstructure, the stringers in the movable span have mod. to heavy corr. with SL up to

**Resources**

Time to Inspect (H:M): 12:0	10:0	Traffic Control: N	Boat: N	Waders: N	Snooper: N
Ladder: LS Y	Manlift: M Y	Bucket Truck: N	Other: Barge	Barge	

**Inspector's Appraisals**

	Prev	New	Comments
58 – Deck Condition:	6	6	Concrete deck exhibits hairline transverse cracks over a majority of the deck. Steel deck exhibits isolated broken grids.
59 – Superstructure Cond:	4	4	Controlled by fracture critical inspection on 8/11/2021.
60 – Substructure Cond:	5	5	Corrosion has caused moderate section loss up to 10%.
62 – Culvert Condition:	N	N	
61 – Channel Condition:	6	6	West bank is beginning to slump.
71 – Waterway Adequacy:	9	9	
72 – Approach Rdw Align:	6	6	
111 – Pier Navig Protection:	3	3	Timber fenders are in place but in a deteriorated condition.

**90B – Inspector Remarks:**

Steel deck has several welded repair plates throughout, Superstructure stringers and floorbeams exhibit moderate to heavy corrosion and section loss. Lateral bracing exhibits moderate to heavy corrosion, severed lattice members, and bent flanges.



**Illinois Department of Transportation  
Structures Information Management System  
Inventory Turnaround Report (S-105)**

Date: 10/6/2021

**Structure Number: 016-6038**

District: 1      Maintenance County: COOK      Municipality: CHICAGO      Bridge Status: OPEN - LOAD POSTED  
 Maintenance Township: HYDE PARK (CHICAGO)      Status Date: 1/10/2013  
 Key Route On: FEDERAL-AID PRIMARY 0029      Sta: 15.9400      Seg:      Spur/Alt: Main Route      Sufficiency Rating: 47.3  
 Key Rt Under:      Sta:      Seg:      Spur/Alt:      HBP Eligible: Yes

\*\*\*\*\* Screen 1 \*\*\*\*\*

Item No. / Name	Existing Values	Revisions	Item No. / Name	Existing Values	Revisions
(7) Facility Carried: FAP-29 (95TH ST)			(101) Parallel Designation: N		
(6) Feature Crossed: Calumet River			Parallel SN:		
(9) Location: 3258 E.95TH STREET			(8E) Replaced By Struct Number:		
(7A) Bridge Name: 95TH STREET BRIDGE			(8D) Replaces Structure Number:		
(3B) Maintenance County: 016			(49) Structure Length (Ft.): 343.0		
(3B1) Maintenance Township: 60			(112) AASHTO Bridge Length (Ft.): 99.9		
(21) Maintenance Resp: 40			(51) Bridge Roadway Width (Ft.): 62.0		
Other Resp:			(32) Approach Roadway Width (Ft.): 76.0		
Other Sec Resp:			(52) Deck Width (Ft.): 87.0		
(42) Service On/Under: 15			(107/A) Deck Type/Thickness (In.): G 5.0		
Other Service On:			Other Deck Type:		
Other Service Under:			(48) Length of Longest Span (Ft.): 238.5		
(22A) Reporting Agency: 4			(45/6) Nbr Spans Main/Approach: 1 4		
Other Reporting Agcy:			(43A/B) Main Span Material/Type: 3 16		
(20) Toll Facility: 0			Other Span Material:		
(35) Structure Flared: 0			Other Span Type:		
(31) Design Load: 02			(44AN/BN) Near Appr Span Matrl/Type #1: 3 02		
(31A) Struct Steel Weight (Lbs.): 0			(44AN/BN) Near Appr Span Matrl/Type #2:		
(60A/B) Substr Matrl: 53			(44AF/BF) Far Appr Span Matrl/Type #1: 3 02		
(8A1) Bridge Remarks (Existing):			(44AF/BF) Far Appr Span Matrl/Type #2:		
			Bridge Remarks (Revised):		

\*\*\*\*\* Screen 2 \*\*\*\*\*

Item No. / Name	Existing Values	Revisions	Item No. / Name	Existing Values	Revisions
(34A) Skew Dir/Angle (DEG): N / 0			(202) Traffic Permits Rte Sec Nbr:		
(33) Bridge Median Type: 0			(8B) Multi-Level Structure Number:		
(33A) Bridge Median Width (Ft): 0			(62A) Culvert Cells (Count): 0		
(38) Navigation Control: 1			(62B) Culvert Cell Width (Ft.): 0.00		
(39) Navigation Vert Clear (Ft): 20			(62C) Culvert Cell Height (Ft.): 0.00		
(40) Navigation Horiz Clea (Ft): 170			(62D) Culvert Opening Area (Sq. Ft.): 0.0		
(50A) Sidewalk Width On - Right (Ft): 8.7			(62E) Culvert Fill Depth (Ft.): 0.0		
(50B) Sidewalk Width On - Left (Ft): 8.7			(16) Latitude: 41.72287710		
(50C) Sidewalks Under Structure: 0			(17) Longitude: 87.54368912		
(36E) Guardrails On - Right: 0			(98A) Border Bridge State Number:		
Other Guardrail Right:			(98B) BorderBridge Adj State (% Resp): 0		
(36F) Guardrails On - Left: 0			(99) Border Bridge Number Existing:		
Other Guardrail Left:			Border Bridge Remarks (Existing):		
(8C) RR Crossing Numbers:					
(55B1) RR Lateral Underclearance (Ft.): 0.0					
(54B3) RR Vert Underclearance (Ft. - In.): 0 - 0					



**Illinois Department of Transportation  
Structures Information Management System  
Inspector's Inventory Report (S-114)**

Date: 10/6/2021

**Structure Number:** 016-6038

**District:** 1  
**Municipality:** CHICAGO  
**Facility Carried:** FAP-29 (95TH ST)  
**Feature Crossed:** Calumet River  
**(21) Maintenance Resp:** MUNICIPALITY  
**Other Resp:**  
**(22A) Reporting Agency:** MUNICIPALITY  
**Other Reporting Agcy:**  
**(41) Bridge Status:** 2 (OPEN - LOAD POSTED)  
**(41A) Status Date:** 1/10/2013  
**(41B) Status Remarks:**

**Maintenance County:** COOK  
**Maint Township:** HYDE PARK (CHICAGO)  
**Bridge Name:** 95TH STREET BRIDGE  
**Location:** 3258 E.95TH STREET  
**Other Sec Resp:** UNKNOWN

<u>Item No. / Name</u>	<u>Construction Information</u>	
	<u>Original</u>	<u>Existing Values</u>
<b>(27/27A) Year/Type:</b>	1958	<u>Reconstruction</u> 1995
<b>(27B) Route:</b>	FA 127	FAP 002
<b>(27C) Section:</b>	FA 127 1414	
<b>(27D) Station:</b>		
<b>(27E) Contract :</b>		
<b>(27F) Project:</b>	00000000000	
<b>(27G) Built By:</b>	CITY	CITY

**(42) Service On/Under:** 1 HIGHWAY 5 WATERWAY  
**Other Service On:** \_\_\_\_\_ **Other Service Under:** \_\_\_\_\_

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<u>Item No. / Name</u>	<u>Existing Values</u>	<u>Revisions</u>	<u>Item No. / Name</u>	<u>Existing Values</u>	<u>Revisions</u>
<b>(101) Parallel Designation:</b>	N	_____	<b>(49) Structure Length (Ft.):</b>	343.0	_____
<b>Parallel SN:</b>		_____	<b>(112) AASHTO Bridge Length (Ft.):</b>	99.9	_____
<b>(35) Structure Flared:</b>	0	_____	<b>(51) Bridge Roadway Width (Ft.):</b>	62.0	_____
<b>(31) Design Load:</b>	02	_____	<b>(32) Approach Roadway Width (Ft.):</b>	76.0	_____
<b>(31A) Struct Steel Weight (Lbs.):</b>	0	_____	<b>(52) Deck Width (Ft.):</b>	87.0	_____
<b>(60A/B) Substr Matr:</b>	53	_____	<b>(48) Length of Longest Span (Ft.):</b>	238.5	_____
<b>(8A1) Bridge Remarks (Existing):</b>		_____	<b>(107/A) Deck Type/Thickness (In.):</b>	G 5.0	____/____
<b>Bridge Remarks (Revised):</b>		_____	<b>Other Deck Type:</b>		
_____		_____	<b>(45/6) Nbr Spans Main/Approach:</b>	1 4	____/____
_____		_____	<b>(43A/B) Main Span Material/Type:</b>	3 16	____/____
<b>Border Bridge Remarks:</b>		_____	<b>Other Span Material:</b>		
		_____	<b>(44AN/BN) Near Appr Span Matr/Type #1:</b>	3 02	____/____
		_____	<b>(44AN/BN) Near Appr Span Matr/Type #2:</b>		____/____
		_____	<b>(44AF/BF) Far Appr Span Matr/Type #1:</b>	3 02	____/____
		_____	<b>(44AF/BF) Far Appr Span Matr/Type #2:</b>		____/____

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<u>Item No. / Name</u>	<u>Existing Values</u>	<u>Revisions</u>	<u>Item No. / Name</u>	<u>Existing Values</u>	<u>Revisions</u>
<b>(34A) Skew Dir/Angle (DEG):</b>	N / 0	____/____	<b>(36E) Guardrails On - Right:</b>	0	_____
<b>(33) Bridge Median Type:</b>	0	_____	<b>(36F) Guardrails On - Left:</b>	0	_____
<b>(33A) Bridge Median Width (Ft.):</b>	0	_____	<b>(55B1) RR Lateral Underclearance (Ft.):</b>	0.0	_____
<b>(38) Navigation Control:</b>	1	_____	<b>(54B3) RR Vert Underclearance (Ft. - In.):</b>	0 - 0	____-____
<b>(39) Navigation Vert Clear (Ft.):</b>	20	_____	<b>(62A) Culvert Cells (Count):</b>	0	_____
<b>(40) Navigation Horiz Clea (Ft.):</b>	170	_____	<b>(62B) Culvert Cell Width (Ft.):</b>	0.00	_____
<b>(50A) Sidewalk Width On - Right (Ft.):</b>	8.7	_____	<b>(62C) Culvert Cell Height (Ft.):</b>	0.00	_____
<b>(50B) Sidewalk Width On - Left (Ft.):</b>	8.7	_____	<b>(62D) Culvert Cell Opening Area (Sq. Ft.):</b>	0.0	_____
<b>(50C) Sidewalks Under Structure:</b>	0	_____	<b>(62E) Culvert Fill Depth (Ft.):</b>	0.0	_____

**Illinois Department of Transportation  
Structures Information Management System  
Inspector's Inventory Report (S-114)**

Date: 10/6/2021

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<u>Item No. / Name</u>	<u>Key Route On</u>		<u>Revisions</u>	<u>Item No. / Name</u>	<u>Key Route Under</u>		<u>Revisions</u>	
	<u>Existing Values</u>				<u>Existing Values</u>			
(28) Number Of Lanes:		4	___	(28) Number Of Lanes:			___	
(102) One Or Two Way Traffic:		2	___	(102) One Or Two Way Traffic:			___	
	<u>South Or East</u>		<u>North Or West</u>		<u>South Or East</u>		<u>North Or West</u>	
	<u>Value</u>	<u>Revisions</u>	<u>Value</u>	<u>Revisions</u>	<u>Value</u>	<u>Revisions</u>	<u>Value</u>	<u>Revisions</u>
(47) Max Rdwy Width (Ft.):	62.0	___						
(47A/B) Horizontal (Ft.):	65.0	___	0.0	___				
<b>KR On Remarks:</b>				<b>KR Under Remarks:</b>				



SN: 016-6038	District: 1	Spans: 1	Appr. Spans: 4	Skew: 0	ADT: 10000	Truck Pct: 5
ADT Un:	Maint. Co: COOK	Twsp: HYDE PARK (CHICAGO)	Status: OPEN - LOAD POSTED			
Facility Carried: FAP-29 (95TH ST)	Feature Crossed: Calumet River					
Location 3258 E.95TH STREET	Municipality: CHICAGO	Team/Sub: /	Insp/Rte:			
Bridge Name: 95TH STREET BRIDGE	Material & Type: STEEL/MOVEABLE - BASCULE					
Insp. Intervals Routine: 24	Fracture Critical: 12	Underwater: 60	Special: N/A	Element Level: 24		
93D - Inspection Date: 08/11/2021	90C - Temp. (°F) 82					
Is Delinquent: <input type="checkbox"/>	Reason:					
90E - Agency Program Manager: Jurca, V.	90E3 - Consultant Program Manager: Bendok, M.					
90E1 - Team Leader: Simpson, J.	90E2 - Inspector: Behnke, J.					




**Resources**

Time to Inspect (H:M): 12:0	10:0	Traffic Control: <input type="checkbox"/>	N	Boat: <input type="checkbox"/>	N	Waders: <input type="checkbox"/>	N	Snooper: <input type="checkbox"/>	N
Ladder: LS Y	Manlift: M Y	Bucket Truck: <input type="checkbox"/>	N	Other: Barge	Barge				

**Inspector's Appraisals**

Element	Element Description	Env	Quantity	Unit	CS1	CS2	CS3	CS4
8014	Concrete Deck Protected w/ HMA Overlay	4	10833	SF	9,559	1,239	21	14
	Remarks	There are a few overlapping spalls in the deck surface and soffit in Span 5.						
28	Steel Deck Open Grid	4	12400	SF	0	12,380	0	20
	Remarks	There are a few broken steel grids.						
29	Steel Deck Concrete Filled Grid	4	1488	SF	0	0	1,488	0
	Remarks	Spalled and scaled concrete in grid and minor to moderate corrosion of steel grid.						
8058	Sidewalk (SF)	4	8000	SF	6,454	835	8	703
	Remarks	<del>Several sections of sidewalk in Span 3 are heaved up and</del> there are a few through spalls in Spans 4 and 5.						
102	Lead Painted Steel Closed Web/Box Girder	3	5316	SF	3,680	1,084	552	0
	Remarks	Moderate to heavy areas of corrosion with minor to moderate SL at long. girder ends or river pier.						
113	Lead Painted Steel Stringer	4	42931	SF	13,422	14,382	12,930	2,197
	Remarks	Span 3 stringers have minor to mod. corr. with minor to mod. SL throughout and several with corrosion holes.						
8121	Lead Painted Steel Bottom Chord Through Truss	3	9260	SF	3,191	2,778	2,340	951
	Remarks	Minor to moderate corrosion along side plates and webs with minor to moderate SL.						
8126	Lead Painted Steel Thru Truss Excluding Bottom Cho	3	25964	SF	12,975	6,491	5,199	1,299
	Remarks	Areas of moderate corrosion with moderate section loss and several isolated corrosion holes.						
152	Lead Painted Steel Floor Beam	4	17703	SF	3,850	2,884	8,914	2,055
	Remarks	Span 3 floorbeams have minor to mod. corr. with minor to mod. SL throughout and several w/corr. holes.						
162	Lead Painted Gusset Plate	3	184	EA	0	164	20	0
	Remarks	Several gusset plates with minor to moderate corrosion and section loss.						
8178	Lead Painted Steel Stringer Ends Including Diaphra	4	152	EA	34	59	47	12
	Remarks	Roadway/sidewalk stringer ends at the center break have mod. corr.; a few bent stringer ends at the E. rear break						
8191	Lead Painted Steel Floor Beam Below Deck Joints	4	4790	SF	0	2,734	2,056	0
	Remarks	Span 3 floorbeams have minor to mod. corr. with minor to mod. SL throughout.						
202	Lead Painted Steel Column or Pile Extension	1	5830	SF	4,040	1,478	312	0
	Remarks	Minor to moderate corrosion at the base of the anchor columns.						
210	Reinforced Conc Pier Wall	1	7360	SF	6,604	368	368	20
	Remarks	Deep spalls and wide cracks in river piers.						
215	Reinforced Conc Abutment	1	11095	SF	9,466	1,453	176	0
	Remarks	A few spalls with exposed reinforcement on the abutments.						
231	Lead Painted Steel Pier or Abutment Cap	1	2619	SF	2,357	262	0	0
	Remarks							
234	Reinforced Conc Pier or Abutment Cap	1	340	LF	291	24	25	0
	Remarks	A few spalls with exposed reinforcement on the abutments.						
302	Preformed Joint Seal	4	196	LF	0	24	49	123
	Remarks	Compressed gland across the East Abutment joint and missing steel section at the West Abutment joint.						

304	Open Expansion Joint	4	261	LF	0	261	0	0
	Remarks							
311	Movable Discontinuous Brg.	4	44	EA	0	23	21	0
	Remarks	Moderate corrosion and section loss on several bearings at the abutments.						
313	Fixed Bearing	3	16	EA	0	7	9	0
	Remarks	Moderate corrosion and section loss on several bearings.						
8323	Approach Pavement	4	2	EA	0	1	1	0
	Remarks	Wide cracks and large spalls in the West Approach Pavement.						
330	Metal Bridge Railing	4	707	LF	0	354	332	21
	Remarks	Moderate corrosion throughout and sections with large corrosion holes in Span 5.						
8402	Steel Bottom Chord Through Truss	3	4	LF	0	0	4	0
	Remarks	Bent bottom flanges at NE and SE member 12-14 and SE member 0-2.						

	Signature	Date
Inspection Team Leader:		10/ 11 / 2021
Consultant Program Manager:		10 / 12 / 21
Agency Program Manager:		10/ 13 / 2021

8408	Steel Floorbeam	3	124	LF	120	4	0	0
	Remarks	Slight rotation of web at FB16W; diagonal cracks in top corners at both ends of FB19W.						
8410	Steel Pier or Abutment Cap	3	62	LF	56	6	0	0
	Remarks	FB21W exhibits bowing, and web deformation and top flange tears at both ends.						



SN: 016-6038	District: 1	Spans: 1	Appr. Spans: 4	Skew: 0	ADT: 10000	Truck Pct: 5
ADT Un:	Maint. Co: COOK	Twsp: HYDE PARK (CHICAGO)	Status: OPEN - LOAD POSTED			
Facility Carried: FAP-29 (95TH ST)	Feature Crossed: Calumet River					
Location 3258 E.95TH STREET	Municipality: CHICAGO	Team/Sub: /	Insp/Rte:			
Bridge Name: 95TH STREET BRIDGE	Material & Type: STEEL/MOVEABLE - BASCULE					
Insp. Intervals Routine: 24	Fracture Critical: 12	Underwater: 60	Special: N/A	Element Level: 24		

93A – Inspection Date: 08 / 11 / 21	93A4 – Temp. (°F): 82					
Is Delinquent: <input type="checkbox"/>	Reason:					
90A – Agency Program Manager: Jurca, V.	90A3 – Consultant Program Manager: Bendok, M.					
93A3 – Team Leader: Simpson, J.	93A5 – Inspector: Behnke, J.					

**Resources**

Time to Inspect (H:M): 19:45	10:0	Traffic Control: N	Boat: N	Waders: N	Snooper: N	
Ladder: LS	LS	Manlift: M	Y	Bucket Truck: N	Other: Barge	Barge




**Inspector's Appraisals**

92A1 – Type: X3	If "X4-Other" Description:					
93A1 – Rating: Prev 6	New: 6	FC Method: Prev V	New: MP <input type="checkbox"/>	DP <input type="checkbox"/>	UT <input type="checkbox"/>	V <input checked="" type="checkbox"/>
93A2 – Remarks: Trunnion girders have areas of mod. to heavy corr. with min. to mod. SL at the girder ends on the river piers.						

92A1 – Type: X1	If "X4-Other" Description:					
93A1 – Rating: Prev 5	New: 5	FC Method: Prev V	New: MP <input type="checkbox"/>	DP <input type="checkbox"/>	UT <input type="checkbox"/>	V <input checked="" type="checkbox"/>
93A2 – Remarks: Bottom chord member 0-2 has min. to mod. corr. w/min. to mod. SL along the b/flange and webs with scattered isolated areas of corr. holes. Several gusset plates have min. to mod. corr and SL with areas of pack rust and isolated small corr. holes. SE member 0-2 has a weld on the south face at the platform connection, there are several welded attachments along the top chord members, and there are steel railings welded to the faces of the vertical and diagonal members. The inside face flanges of member 15-17 in each quadrant exhibits scrape marks.						

92A1 – Type: X2	If "X4-Other" Description:					
93A1 – Rating: Prev 4	New: 4	FC Method: Prev V-DP	New: MP <input type="checkbox"/>	DP <input type="checkbox"/>	UT <input type="checkbox"/>	V <input checked="" type="checkbox"/>
93A2 – Remarks: Floorbeam 19W has a 1.5" long crack emanating from the top coping in the web at each end, and there is rotation in the web at the same location up to approx. 4.5 degrees to the east. Trunnion Cross Girder 16W exhibits rotation of up to approx. 3 degrees to the east in the web ends just above the b/flange. Movable span floorbeams have min. to mod. corr. with min. to mod. SL throughout and several with small corr. holes in the webs and larger corr. holes around stringer connection locations.						

92A1 – Type: E2	If "X4-Other" Description:					
93A1 – Rating: Prev 5	New: 5	FC Method: Prev V	New: MP <input type="checkbox"/>	DP <input type="checkbox"/>	UT <input type="checkbox"/>	V <input checked="" type="checkbox"/>
93A2 – Remarks: The anchor column floorbeams typ. have minor corr. The West Anchor Column Floorbeam is horizontally displaced to the east (up to 4" at midspan) and there is a rotation in the web up to approx. 6.5 degrees directly below stringers S1 and S12 (web plumb at midspan); the east edge of the bottom cover plate at midspan has 8 divots which line up with rivets in the back face of the counterweight.						

	Signature	Date
Inspection Team Leader:		10 / 11 /2021
Consultant Program Manager:		10 / 12 / 21
Agency Program Manager:		10 / 13 /2021

**Two Girder**

- A1- Suspension Link & Pin
- A2- Suspension Single Pin
- A3- Tension Flanges Riveted/  
Bolted Plate Girders
- A4- Bearing Seat of Suspended  
Spans
- A5- Tension Flange of Rolled  
Beam
- A6- Tension Flange of Welded  
Plate Girders
- A7- Tension Flanges of Lattice  
Truss Web Girders

**Truss System**

- B1- Eyebar & Pin Tension Members
- B2- Simple Span Welded Truss  
Tension Members
- B3- Hanger Link & Pin of Suspended  
Trusses
- B4- Single element Tension Members
- B5- Simple Span riveted/Bolted  
Tension Members
- B6- Continuous Truss System-Welded,  
Riveted or Bolted Tension Members

**Cable Stayed & Suspension**

- C1- Suspension Bridge-Cables
- C2- Cable Stayed-Cables

**Tied Arches**

- D1- Welded Box Ties
- D2- Riveted/Bolted Box Ties
- D3- Stiffened Girders

**Framed Steel Substructure**

- E1- Welded or Rolled Pier Cap
- E2- Riveted or Bolted Pier Cap
- E3- Welded or Rolled Pier Column
- E4- Riveted or Rolled Pier Column

**Box Beams**

- F1- Single Welded Box
- F2- Single Riveted/Bolted Box
- F3- Double Box Beam-Welded,  
Riveted or Bolted

**Other Types**

- X1- Bascule
- X2- Floorbeams supporting other  
steel members or spacing > 15 ft.
- X3- Cross Frames or Transfer  
Beams
- X4- Other

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Structures Information Management System  
Master Structure Report (S-107)**

Date: 10/6/2021

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Structure Number: 016-6038 District: 1

**Inventory Data**

Facility Carried:	FAP-29 (95TH ST)	Bridge Name:	95TH STREET BRIDGE	Sufficiency Rating:	47.3	Structure Length:	343.0
Feature Crossed:	Calumet River	Location:	3258 E.95TH STREET	HBP Eligible:	Yes	AASHTO Bridge Length:	99.9
Bridge Remarks:				Replaced By:		Length of Long Span:	238.5
Bridge Status:	2 OPEN - LOAD POSTED	StatusDate:	1/10/2013	Replaces:		Bridge Roadway Width:	62.0
Status Remarks:				Last Update Date:	03/30/2021	Appr Roadway Width:	76.0
Maint County:	016 COOK	Maint Township:	60 HYDE PARK (CHICAGO)	Parallel Structure:	None	Deck Width:	87.0
Maint Responsibility:	40 MUNICIPALITY		UNKNOWN	Multi-Level Structure Nbr:		Sidewalk Width Right:	8.7
Service On/Under:	1 HIGHWAY / 5 WATERWAY			Skew Direction:	None	Sidewalk Width Left:	8.7
Reporting Agency:	4 MUNICIPALITY			Skew Angle:	0 D	Navigation Control:	1 Yes
Main Span Matl/Type:	3 STEEL / 16 MOVEABLE - BASCULE			Structure Flared:	No	Navigation Horiz Clear:	170
Nbr Of Main Spans:	1	Nbr Of Approach Spans:	4	Historical Significance:	No	Navigation Vert Clear:	20
***Approaches***				Border Bridge State:		Culvert Fill Depth:	0.0
Near #1 Matl/Type:	3 STEEL / 02 STRINGER/MULTI-BEAM/GIRDER			Bdr State SN:		Number Culvert Cells:	0
Near #2 Matl/Type:				Bdr State % Responsibility:	0	Culvert Opening Area:	0.0
Far #1 Matl/Type:	3 STEEL / 02 STRINGER/MULTI-BEAM/GIRDER			Structural Steel Wt:	0	Culvert Cell Height:	0.00
Far #2 Matl/Type:				Substructure Material:	53	Culvert Cell Width:	0.00
Median Width/Type:	0 Ft / 0 None			Rated By:	2 IDOT	Rate Method:	6 LOAD FACTOR (LF) REP
Guardrail Type L/R:	0 None / 0 None	Inventory Rating:	0.535 (19)	Load Rating Date:	01/01/2010	***Railroad Crossing Info***	
Toll Facility Indicator:	0 No Toll	Operating Rating:	0.890 (32)			Crossing 1 Nbr:	
Latitude:	41.72287710	Longitude:	87.54368912	Design Load:	02 HS20	Crossing 1 Nbr:	
Deck Structure Type:	G OPEN STEEL GRATING	Deck Structure Thickness:	5.0	SD:	Y	FO:	N
Sidewalks Under Structure:	0 None					RR Lateral Underclear:	0.0
						RR Vertical Underclear:	0 Ft 0 In

**Key Route On Data**

Key Route Nbr:	FEDERAL-AID PRIMARY	0029	Station:	15.9400
Appurtenances	Main Route	00000	Segment:	
Inventory County:	016 COOK		Linked:	Y
Township/Road Dist:	60 HYDE PARK (CHICAGO)	Natl. Hwy System:	On NHS	
Municipality:	1051 CHICAGO	Inventory Direction:		
Urban Area:	1051	Curr AADT Yr/Count:	2019 / 10000	
Functional Class:	3 OTHER PRINCIPAL ARTERIAL	Est Truck Percentage:	5 %	
** CLEARANCES **	South/East	North/West	Number Of Lanes:	4
Max Rdwy Width:	62.0		One Or Two Way:	2 Two-Way
Horizontal:	65.0	0.0	Bypass Length:	1
Min Vertical:	99Ft 11In	00Ft 00In	Future AADT Yr/Cnt:	2032 / 12154
10 Ft Vertical:	99Ft 11In	00Ft 00In	Designated Truck Rte:	CLASS II
Lateral:			Special Systems:	Yes

**Key Route Under Data**

		Station:	
		Segment:	
		Linked:	
		Natl. Hwy System:	
		Inventory Direction:	
		Curr AADT Yr/Count:	/
		Est Truck Percentage:	%
		Number Of Lanes:	
		One Or Two Way:	
		Bypass Length:	
		Future AADT Yr/Cnt:	/
		Designated Truck Rte:	
		Special Systems:	

\*\*\* Marked Route On Data \*\*\*

	Designation	Kind	Number
Route #1:	1 Mainline	2 U.S. Highways	012
Route #2:	1 Mainline	2 U.S. Highways	020
Route #3:	1 Mainline		

\*\*\* Marked Route Under Data \*\*\*

	Designation	Kind	Number

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Master Structure Report (S-107)**

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Structure Number: 016-6038 District: 1

**Data Related to Inspection Information**

\*\*\*Inspection Intervals\*\*\*  
 Routine NBIS:  MOS Underwater:  MOS  
 Fracture Critical:  MOS Special:   
 \*\*\* Maximum Allowable Posting Limits \*\*\*  
 One Truck At A Time:  Tons  
 Single Unit Vehicles:  Tons  
 Combination Type 3S-1:  Tons  
 Combination Type 3S-2:  Tons  
 Bridge Posting Level:  < 10% Below Legal Loads

**Inspection/Appraisal Information**

Inspection Date:	<input type="text" value="08/06/2019"/>	Inspection Temperature:	<input type="text" value="69"/> Deg. F	Insp by (Name):	<input type="text" value="Vaicik, Stephen"/>	** Actual Posted Limits **
Deck:	<input type="text" value="6"/>	SATISFACTORY CONDITION - MINOR DETERIORATION		Insp by (Name):	<input type="text" value="Moreno, B.S."/>	Single Unit Vehicles: <input type="text" value="22"/> Tons
Superstructure:	<input type="text" value="4"/>	POOR CONDITION - ADVANCED DETERIORATION		Utilities Attached:	<input type="text" value="9"/> ELECTRIC	Combination Type 3S-1: <input type="text" value="29"/> Tons
Substructure:	<input type="text" value="5"/>	FAIR CONDITION - MINOR SECTION LOSS, CRACKS			<input type="text" value="N"/> N/A	Combination Type 3S-2: <input type="text" value="36"/> Tons
Culvert:	<input type="text" value="N"/>	NOT APPLICABLE			<input type="text" value="N"/> N/A	One Truck At A Time: <input type="text" value="0"/>
Channel and Protection:	<input type="text" value="6"/>	SATISFACTORY CONDITION - MINOR DETERIORATION		Deck Wearing Surf:	<input type="text" value="P"/> GRATING	<b>Last Paint Type:</b>
Structural Evaluation:	<input type="text" value="4"/>	MINIMUM ADEQUACY TO BE LEFT IN PLACE		Deck Membrane:	<input type="text" value="F"/> NONE	<input type="text" value="IZ"/>
Deck Geometry:	<input type="text" value="6"/>	EQUAL TO PRESENT MINIMUM CRITERIA		Deck Protection:	<input type="text" value="J"/> NONE	
Underclearance-Vert/Lat.:	<input type="text" value="N"/>	NOT APPLICABLE		Total Deck Thick:	<input type="text" value="5.0"/>	
Waterway Adequacy:	<input type="text" value="9"/>	SUPERIOR TO PRESENT DESIRABLE CRITERIA		Last Paint Date:	<input type="text" value="10/2012"/>	
Approach Roadway Align:	<input type="text" value="6"/>	EQUAL TO PRESENT MINIMUM CRITERIA		Inspection Remarks:	Joint Openings (In.) W. Abut., 1.5"; E. Abut., 1 3/8" (measured @ N. shoulder) Deck, the open steel grating has several welded repair plates throughout and the conc. filled grid has wide spread spalls and scaling along the surface and s	
Bridge Railing Appraisal:	<input type="text" value="2"/>	Doesn't Meet Standards				
Approach Guardrail:	<input type="text" value="111"/>	Does Not Exist	<input type="text" value="Does Not Exist"/>			
Pier Navig Protection:	<input type="text" value="3"/>	IN PLACE BUT IN A DETERIORATED CONDITION				

**Underwater Inspection/Appraisal Information**

Inspection Date:   
 Temperature:  Inspection Method:  Diver  Sonar   
 Inspected By:  Inspected By:  Appraisal Rating:  FAIR CONDITION  
 Inspection Remarks: CONCRETE SCALING, SECTION LOSS, CRACKS, AND POOR CONSOLIDATION AT BOTH PIERS. UNDERMINING WITH EXPOSED CONCRETE CAISSONS AT BOTH PIERS. MISSING AND DAMAGED TIMBER FENDERS AT BOTH PIERS.

**Scour Critical Information**

Rating:  CALCULATED SCOUR ABOVE FOOTING Evaluation Method:  Rational Analysis  
 Analysis Date:  Analysis By:

**Miscellaneous**

Fracture Critical Members: Yes  
 Microfilm Data Recorded: No

**Construction Information**

Year:	<input type="text" value="1958"/> Original	<input type="text" value="1995"/> Reconstructed
Route:	<input type="text" value="FA 127"/> Sta: <input type="text"/>	<input type="text" value="FAP 002"/> Sta: <input type="text"/>
Section Nbr:	<input type="text" value="FA 127 1414"/>	
Contract Nbr:	<input type="text"/>	
Fed Aid Pr #:	<input type="text" value="000000000000000"/>	
Built By:	<input type="text" value="4"/> CITY	<input type="text" value="4"/> CITY

**Proposed Improvement**

Cost Estimate Year:	<input type="text" value="2000"/>	Length:	<input type="text" value="360"/>	*** Costs in Dollars ***
Type of Work:	<input type="text" value="31"/> REPLACEMENT DUE TO SUBSTANDARD CAPACITY OR GEOMETRICS			Bridge Cost: <input type="text" value="1,902"/>
Done By:	<input type="text" value="1"/> Contract			Roadway Cost: <input type="text" value="190"/>
Remarks:	<input type="text"/>			Total Project Cost: <input type="text" value="2,853"/>

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Structures Information Management System  
Structure Summary Report**

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Structure Number: 016-6038

District: 1

**Inventory Data**

<b>Facility Carried:</b> 95TH ST	<b>Bridge Name:</b> 95TH STREET BRIDGE	<b>Sufficiency Rating:</b> 47.4	<b>Structure Length:</b> 343.0
<b>Feature Crossed:</b> Calumet River	<b>Location:</b> 3258 E.95TH STREET	<b>HBP Eligible:</b> Yes	<b>AASHTO Bridge Length:</b> 99.9
<b>Bridge Remarks:</b>		<b>Replaced By:</b> -	<b>Length of Long Span:</b> 238.5
<b>Bridge Status:</b> 2 OPEN - LOAD POSTED	<b>Status Date:</b> 1/10/2013 12:00:00 AM	<b>Replaces:</b> -	<b>Bridge Roadway Width:</b> 62.0
<b>Status Remarks:</b> Postings 22/29/36		<b>Last Update Date:</b> 04/04/2022	<b>Appr Roadway Width:</b> 76.0
<b>Maint County:</b> 016 COOK	<b>Maint Township:</b> 60 HYDE PARK (CHICAGO)	<b>Parallel Structure:</b> None	<b>Deck Width:</b> 87.0
<b>Maint Responsibility:</b> 40 MUNICIPALITY	UNKNOWN	<b>Multi-Level Structure Nbr:</b>	<b>Sidewalk Width Right:</b> 8.7
<b>Service On/Under:</b> 1 HIGHWAY	5 / WATERWAY	<b>Skew Direction:</b> N	<b>Sidewalk Width Left:</b> 8.7
<b>Reporting Agency:</b> 4 MUNICIPALITY		<b>Skew Angle:</b> 0 D	<b>Navigation Control:</b> 1 Yes
<b>Main Span Matl/Type:</b> 3 STEEL	/ 16 MOVEABLE - BASCULE	<b>Structure Flared:</b> No	<b>Navigation Horiz Clear:</b> 170
<b>Nbr Of Main Spans:</b> 1	<b>Nbr Of Approach Spans:</b> 4	<b>Historical Significance:</b> No	<b>Navigation Vert Clear:</b> 20
<b>***Approaches***</b>		<b>Border Bridge State:</b>	<b>Culvert Fill Depth:</b> 0.0
<b>Near #1 Matl/Type:</b> 3 STEEL	/ 02 STRINGER/MULTI-BEAM/GIRDER	<b>Bdr State SN:</b>	<b>Number Culvert Cells:</b> 0
<b>Near #2 Matl/Type:</b>	/	<b>Bdr State % Responsibility:</b> 0	<b>Culvert Opening Area:</b> 0.0
<b>Far #1 Matl/Type:</b> 3 STEEL	/ 02 STRINGER/MULTI-BEAM/GIRDER	<b>Structural Steel Wt</b> 0	<b>Culvert Cell Height:</b> 0.00
<b>Far #2 Matl/Type:</b>	/	<b>Substructure Material:</b> 53	<b>Culvert Cell Width:</b> 0.00
<b>Median Width/Type:</b> 0 Ft. / 0 None		<b>Rated By:</b> 2 IDOT	<b>Rate Method:</b> 6
			LOAD FACTOR (LF) REPORTED BY RATING FACTOR (RF)
<b>Guardrail Type L/R:</b> 0None / 0 None	<b>Inventory Rating:</b> 0.535(19)	<b>Load Rating Date:</b> 01/01/2010	<b>Railroad Crossing Info</b>
<b>Toll Facility Indicator:</b> 0 No Toll	<b>Operating Rating:</b> 0.890(32)		<b>Crossing 1 Nbr:</b>
<b>Latitude:</b> 41.72287710	<b>S Longitude:</b> 87.54368912	<b>S Design Load:</b> 02 HS20	<b>Crossing 1 Nbr:</b>
<b>Deck Structure Type:</b> G OPEN STEEL GRATING	<b>Deck Structure Thickness:</b> 5	<b>SD:</b> Y <b>FO:</b> N	<b>RR Lateral Underclear:</b> 0.0
<b>Sidewalks Under Structure:</b> 0 None			<b>RR Vertical Underclear:</b> 0 Ft 0 In

**Key Route On Data**

<b>Key Route Nbr:</b> FEDERAL-AID PRIMARY 0029	<b>Station:</b> 15.9400
<b>Appurtenances</b> Main Route 00000	<b>Segment:</b>
<b>Inventory County:</b> 016 COOK	<b>Linked:</b> Y
<b>Township/Road Dist</b> 60 HYDE PARK (CHICAGO)	<b>Natl. Hwy System:</b> On NHS
<b>Municipality</b> 1051 CHICAGO	<b>Inventory Direction:</b>
<b>Urban Area:</b> 1051 1051	<b>Curr AADT Yr/Count:</b> 2021 / 9300
<b>Functional Class:</b> 3 OTHER PRINCIPAL ARTERIAL	<b>Est Truck Percentage:</b> 3
<b>** CLEARANCES **</b> South/East North/West	<b>Number Of Lanes:</b> 4
<b>Max Rdwy Width:</b> 62.0	<b>One Or Two Way:</b> 2 Two-Way
<b>Horizontal:</b> 65.0 0.0	<b>Bypass Length:</b> 1
	<b>Future AADT Yr/Cnt:</b> 2032 / 12154
	<b>Designated Truck Rte:</b> CLASS II
<b>Lateral:</b>	<b>Special Systems:</b> Yes

**Key Route Under Data**

<b>Station:</b>
<b>Segment:</b>
<b>Linked:</b>
<b>Natl. Hwy System:</b>
<b>Inventory Direction:</b>
<b>Curr AADT Yr/Count:</b> /
<b>Est Truck Percentage:</b>
<b>Number Of Lanes:</b>
<b>One Or Two Way:</b>
<b>Bypass Length:</b>
<b>Future AADT Yr/Cnt:</b> /
<b>Designated Truck Rte:</b>
<b>Special Systems:</b>

**\*\*\* Marked Route On Data \*\*\***

Designation	Kind	Number
Route #1: 1 Mainline	2 U.S. Highways	012
Route #2: 1 Mainline	2 U.S. Highways	020
Route #3: 1 Mainline		

**\*\*\* Marked Route Under Data \*\*\***

Designation	Kind	Number
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**Illinois Department of Transportation  
Structures Information Management System  
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Structure Number: 016-6038

District: 1

**Data Related to Inspection Information**

\*\*\* Inspection Intervals \*\*\*

\*\*\* Maximum Allowable Posting Limits \*\*\*

Bridge Posting Level:

<b>Routine NBIS:</b>	24 MOS	<b>Underwater:</b>	60 MOS	<b>One Truck At A Time:</b>	0	<b>Combination Type 3S-1:</b>	29 Tons	4	< 10% Below Legal Loads
		<b>Special:</b>	N	<b>Single Unit Vehicles:</b>	22 Tons	<b>Combination Type 3S-2:</b>	36 Tons		

**Inspection/Appraisal Information**

<b>Inspection Date:</b>	08/11/2021	<b>Inspection Temperature:</b>	82Deg. F	<b>** Actual Posted Limits **</b>		
<b>Deck:</b>	6	SATISFACTORY CONDITION - MINOR DETERIORATION			<b>Single Unit Vehicles:</b>	22 Tons
<b>Superstructure:</b>	4	POOR CONDITION - ADVANCED DETERIORATION			<b>Combination Type 3S-1:</b>	29 Tons
<b>Substructure:</b>	5	FAIR CONDITION - MINOR SECTION LOSS, CRACKS			<b>Combination Type 3S-2:</b>	36 Tons
<b>Culvert:</b>	N	NOT APPLICABLE			<b>One Truck At A Time:</b>	0
<b>Channel and Protection:</b>	6	SATISFACTORY CONDITION - MINOR DETERIORATION	<b>Deck Wearing Surf:</b>	P GRATING	<b>Last Paint Type:</b> IZ	
<b>Structural Evaluation:</b>	4	MINIMUM ADEQUACY TO BE LEFT IN PLACE	<b>Deck Membrane:</b>	F NONE	ALUM EPOXY MASTIC	
<b>Deck Geometry:</b>	6	EQUAL TO PRESENT MINIMUM CRITERIA	<b>Deck Protection:</b>	J NONE	FIELD O Z E&P	
<b>Underclearance-Vert/Lat.:</b>	N	NOT APPLICABLE	<b>Total Deck Thick:</b>	5.0		
<b>Waterway Adequacy:</b>	9	SUPERIOR TO PRESENT DESIRABLE CRITERIA	<b>Last Paint Date:</b>	10/2012		
<b>Approach Roadway Align:</b>	6	EQUAL TO PRESENT MINIMUM CRITERIA				
<b>Bridge Railing Appraisal:</b>	2	Doesn't Meet Standards				
<b>Approach Guardrail:</b>	111	Does Not Exist	Does Not Exist	Does Not Exist		
<b>Pier Navig Protection:</b>	3	IN PLACE BUT IN A DETERIORATED CONDITION				

**Underwater Inspection/Appraisal Information**

<b>Inspection Date:</b>	06/04/2019						
<b>Temperature:</b>	60	<b>Inspection Method:</b>	DPSV	Diver	Probe	Sonar	Visual
		<b>Appraisal Rating:</b>	5	FAIR CONDITION			

**Scour Critical Information**

**Miscellaneous**

<b>Rating:</b>	8	CALCULATED SCOUR ABOVE FOOTING	<b>Evaluation Method:</b>	B	Rational Analysis	
<b>Analysis Date:</b>	08/22/1997					<b>Microfilm Data Recorded:</b> No

**Construction Information**

<b>Year:</b>	1958	Original	1995	Reconstructed
<b>Route:</b>	FA 127	<b>Sta:</b>	FAP 002	<b>Sta:</b>
<b>Section Nbr:</b>	FA 127 1414			
<b>Contract Nbr:</b>				
<b>Fed Aid Pr#:</b>	00000000000000			
<b>Built By:</b>	4	CITY	4	CITY